

**SAN FRANCISCO BAY AREA RAPID TRANSIT DISTRICT**

2150 Webster Street, Oakland, CA 94612 • P.O. Box 12688, Oakland, CA 94604-2688  
510-464-6000

**NOTICE OF MEETING AND AGENDA  
BART Bicycle Advisory Task Force (BBATF)**

April 6, 2026  
6:00 p.m. – 8:00 p.m.

BBATF Members: Jeremiah Maller (Chairperson), Jon Spangler (Vice Chair), Tyler Morris (Secretary), Al Park, Alex Shu, Ian Gaerlan, Jenn Koscielniak, Jonathan MacMillan, Maya Chaffee, Morris Gevirtz, Paul Valdez, Phoenix Magnum, Sam Greenberg.

Chairperson Jeremiah Maller has called a meeting of the BART Bicycle Advisory Task Force on April 6, 2026, at 6:00 p.m. Public participation for this meeting will be via teleconference only. Presentation materials will be available via Legistar at <https://bart.legistar.com>

You may join the Task Force meeting via Zoom by calling (833) 548-0282 and entering access code 894 1673 0358, logging into Zoom.com and entering access code 894 1673 0358, or typing the following Zoom link into your web browser: <https://us06web.zoom.us/j/89416730358>

**If you wish to make a public comment:**

- 1) Submit written comments via email to [hmaddox@bart.gov](mailto:hmaddox@bart.gov) using “public comment” as the subject line. Your comment will be provided to the Task Force and will become a permanent part of the file. Please submit your comments as far in advance as possible. Emailed comments must be received before noon on April 3, 2026 to be included in the record.
- 2) Call (833) 548-0282, enter access code 894 1673 0358 dial \*9 to raise your hand when you wish to speak, and dial \*6 to unmute when you are requested to speak; log into Zoom.com, enter access code 894 1673 0358 and use the raise hand feature; or join the Task Force meeting via the Zoom link (<https://us06web.zoom.us/j/89416730358>) and use the raise hand feature.

Public comment is limited to two (2) minutes per person.

BART provides services/accommodations upon request to persons with disabilities and individuals who are limited English proficient who wish to address Committee matters. A request must be made between one and five days in advance of Board/Committee meetings, depending on the service requested. Please contact the Office of the District Secretary at (510) 464-6083 for information.

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**AGENDA**

- |     |   |    |      |
|-----|---|----|------|
| 1.  | Self-Introductions of Members, Staff, and Guests (Information)              | 5  | min. |
| 2.  | Public Comment on Non-Agenda Items (Information)                            | 5  | min. |
| 3.  | Approval of February 2026 BBATF Minutes (Action)                            | 5  | min. |
| 4.  | 2026 BBATF Officer Elections (Action)                                       | 15 | min. |
| 5.  | Adeline Quick Build Project (Information)                                   | 25 | min. |
| 6.  | El Cerrito Plaza BART Intersection Safety (Discussion)                      | 20 | min. |
| 7.  | El Cerrito Plaza BART Transit Oriented Development Bike Access (Discussion) | 15 | min. |
| 8.  | West Oakland TOD Update (Information)                                       | 15 | min. |
| 9.  | BART Bike Program Updates (Information)                                     | 10 | min. |
| 10. | Future Agenda Items (Discussion)  | 5  | min. |

**TOTAL: 120 min.**

**SAN FRANCISCO BAY AREA RAPID TRANSIT DISTRICT**  
2150 Webster Street, Oakland, CA 94612 • P.O. Box 12688, Oakland, CA 94604-2688  
510-464-6000

**MINUTES**  
**BART Bicycle Advisory Task Force (BBATF)**  
February 2, 2026 6:00 p.m. – 8:00 p.m.

BBATF Members: Jeremiah Maller (Chairperson), Jon Spangler (Vice Chair), Tyler Morris (Secretary), Al Park, Alex Shu, Ian Gaerlan, Jenn Koscielniak, Maya Chaffee, Morris Gevirtz, Paul Valdez, Phoenix Magnum, Sam Greenberg.

Meeting called to order by Chairperson Jeremiah Maller at 6:05pm

Members in attendance:

Jeremiah Maller (Chair), Jon Spangler (Vice Chair), Tyler Morris (Secretary), Al Park, Maya Chaffee, Morris Gevirtz, Paul Valdez, Sam Greenberg, Alex Shu, Elena O'Curry, Jenn Koscielniak, Ian Gaerlan,

BART Director: Robert Raburn  
BART Liaison: Heath Maddox

Guests: Patrica Schader

Absent: Jonathan MacMillan, Phoenix Magnum

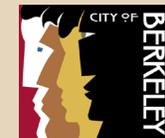
Agenda with minutes follows as is:

1. 6:05pm Self-Introductions of Members, Staff, and Guests (Information) 5 min.
  1. Attendees introduce themselves
2. 6:10pm General Discussion and Public Comment (Information) 5 min.
  1. Director Raburn updated the status of BART funding
  2. Paul mentioned the upcoming Ride of Silence
  3. Morris inquired about BART communications during impacted train status
3. 6:17pm Approval of December 2025 BBATF Minutes (Action) 5 min.
  1. Jon motioned to approve the December minutes and Morris seconded the motion
  2. Motion passed unanimously
4. 6:20pm BBATF Memberships (Action) 10 min.
  1. Jon motioned to re-nominate Jeremiah and Maya to the BBATF and Paul seconded the motion
  2. Motion passed unanimously
5. 6:21pm 2026 BBATF Officer Elections (Action) 10 min.
  1. Jeremiah recaps the current officer status
  2. Maya asked for an overview of officer responsibilities

1. Jeremiah gave an overview of duties and time commitment
3. Jeremiah motions to postpone elections until April and Jon seconds the motion
4. Motion passes unanimously
6. 6:39pm 2024 BART Station Profile Study (Information) 10 min.
  1. Heath introduces the presentation
  2. The survey is conducted every ten years
  3. Comprises fundamental data of cyclists using BART
    1. Home origin to BART
    2. Type of bike
    3. Etc
  4. Jeremiah asks if station maps will be provided for the full station list
  5. Paul asked how this data is used in real-world station planning
    1. Heath said that this data can help to dictate how much bike parking gets installed
  6. Maya asked about the weighted map dots
  7. Tyler asked about the difference in the number of respondents from the prior survey to this survey
  8. Al asked if there is data on micro-mobility for the prior survey
    1. Heath mentioned that many of those micro-mobility options didn't exist then
7. 7:03pm El Cerrito Plaza BART Intersection Safety (Discussion) 15 min.
  1. Morris introduces the presentation
  2. Jeremiah added some background that since a cyclist was hit at the intersection, different improvements have been suggested for the intersection
  3. Jon suggested BBATF drafts a letter including the map Morris made to help offer some suggestions for intersection improvements
8. 7:28pm Next Generation Faregate Reliability and Access (Discussion) 15 min.
  1. Jon introduces the presentation
  2. Jon would like to draft a letter in collaboration with the BART Accessibility Task Force to request BART conduct an accessibility study of the new fare gates
  3. Sam mentioned that the malfunctions can impact the entire transit trip of an accessibility rider
  4. Jon motions to draft the letter and Jeremiah seconds the motion.
  5. Motion passes unanimously.
9. 7:45pm Subcommittee Reports (Information) 5 min.
  1. Advocacy
    1. Jon discussed the cyclist win in Castro Valley
10. 7:47pm Strategic Planning/End of Year Reflection Follow-up (Discussion) 15 min.
  1. Jeremiah briefly recapped some proposals for process changes
  2. Ian suggested tabling as a priority this year
  3. Topic is tabled for further discussion next meeting
11. 7:51pm BBATF Promotional Giveaway Items (Discussion) 5 min.
  1. Topic is tabled for further discussion next meeting
12. 7:52pm BART Bike Program Updates (Information) 5 min.
  1. Heath introduces the presentation
  2. Heath highlights the Adeline Quick Build project
  3. Stairways channel project is progressing
  4. Same group of vandals have been hitting the bike lockers over night

13. 8:05pm Future Agenda Items (Discussion) 5 min.
  1. Follow Up Item: El Cerrito Plaza & EC Del Norte Profile Study maps (and maps for many other stations)
  2. Officer Elections
  3. Strategic Planning (End of Year Reflection Follow-up - Action Item)
  4. El Cerrito Plaza BART Intersection Safety follow up
  5. Promotional Giveaways
  6. Accessibility Letter update
  7. Website update
  8. Task Force recruitment
  9. Presentation on CalSta grant
  10. Berkeley Quick Build update (Adeline)
  11. El Cerrito Plaza TOD

Meeting adjourned at 8:09p.m. by Chairperson Jeremiah Maller  
Next meeting is called by Chairperson Jeremiah Maller on April. 6th, 2026 at 6:00p.m.



# Adeline Quick Build Project

Community Meeting

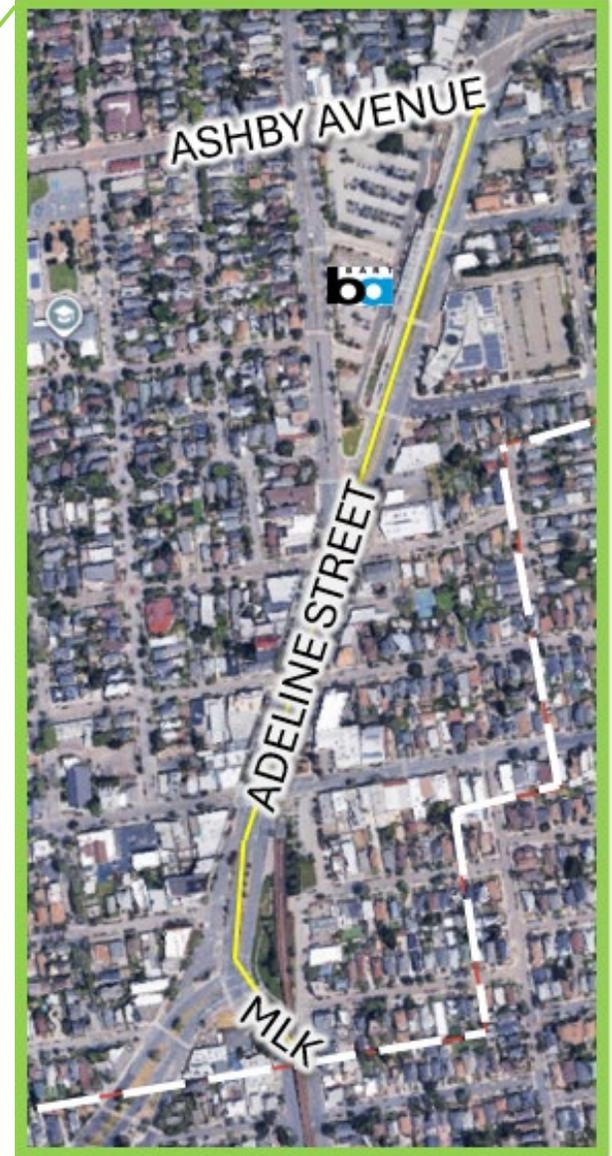
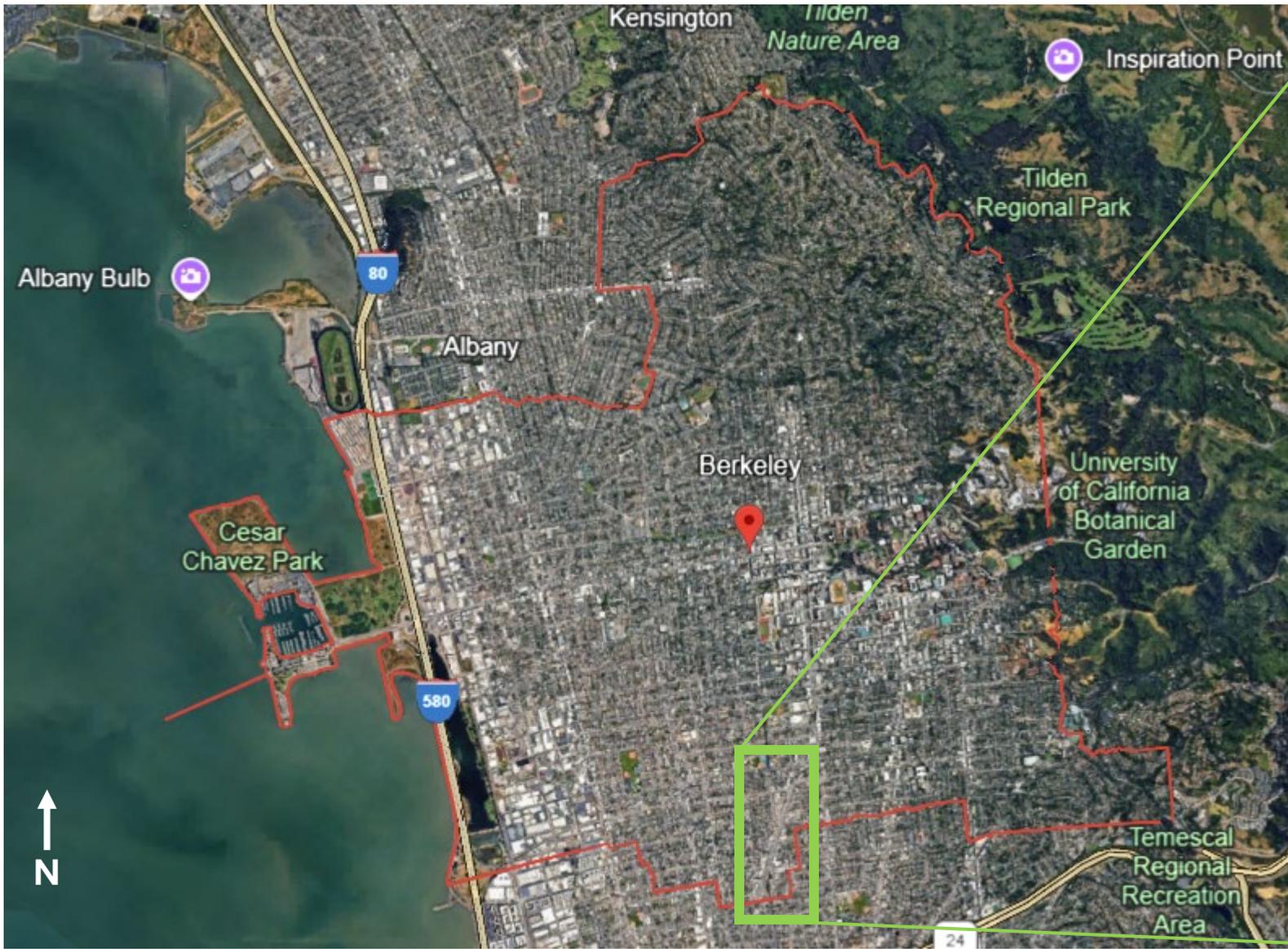
January 21, 2026

# Agenda

- Project Context
- Project Elements
- Questions & Answers



# Project Context



Adeline Quick Build

# Adeline Street History



Key System 1903-1948



BART Construction 1967

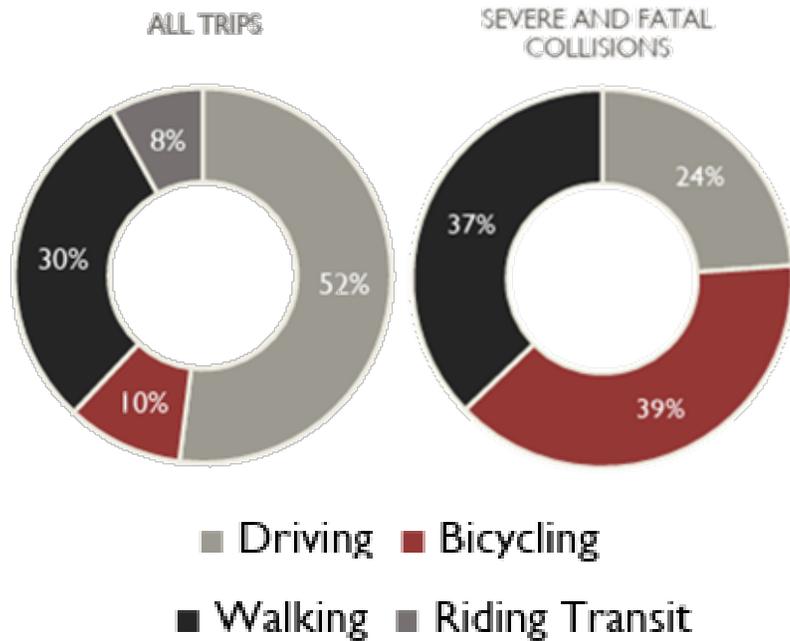
# Present Day Roadway



2 Vehicle Lanes & 1 Bike Lane

3 Vehicle Lanes

# Adeline: High-Injury Street



# Vehicle Speeds



Faster speeds increase chance of fatality for pedestrian when struck by a vehicle

Faster speeds reduce driver field of view and increase the risk of a collision



# Road-Diet Coming to Oakland



# Berkeley Projects & Studies

- 2020 Vision Zero Action Plan
- 2020 Adeline Corridor Specific Plan
- 2020 North Adeline Quick Build Project
- 2022 Council Approved Road-Diet/Plaza next to BART TOD
- **Now - Adeline Quick Build Project**
- *Future - Adeline Transportation Improvements Project*



# Project Goals

- Reduce vehicle speeds
- Increase pedestrian safety, especially at unsignalized intersections
- Reduce conflicts between vehicles and pedestrians/transit riders/cyclists
- Fill bike lane gaps
- Inform the permanent design

## LEGEND

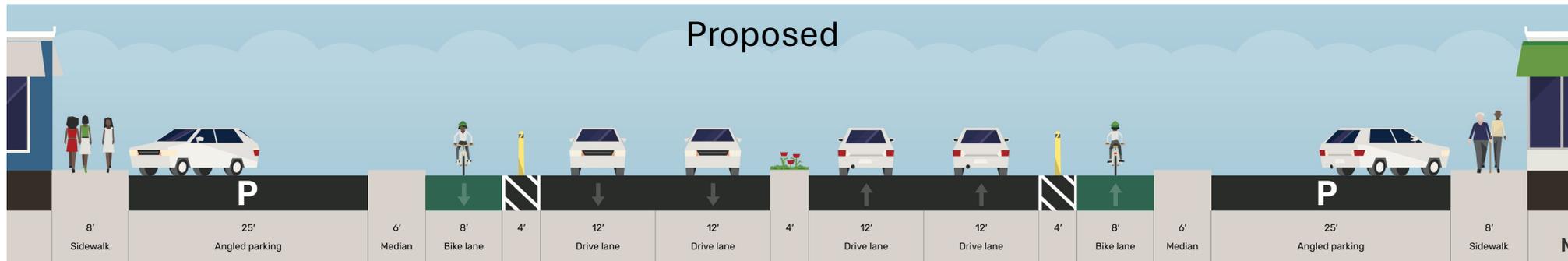
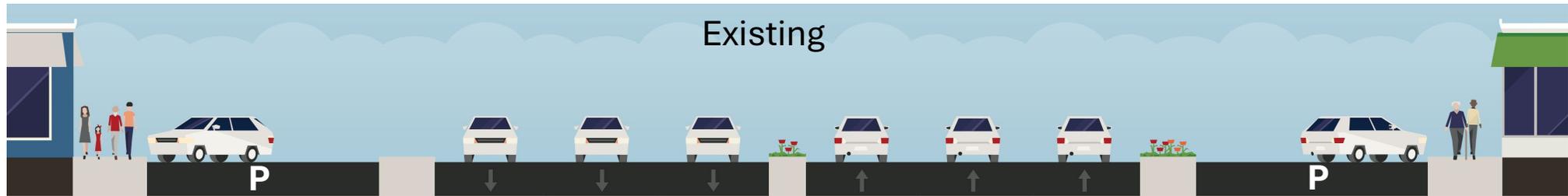
- Project Limits
- - - - City Boundary





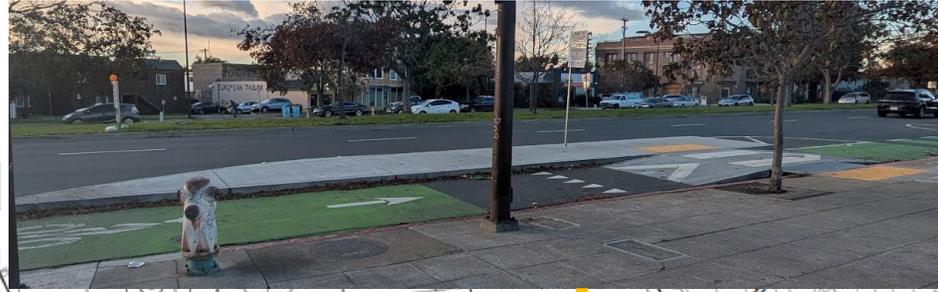
# Project Elements

# Adeline (Oakland - MLK)





# Quick-Build Concept– Alcatraz to Fairview

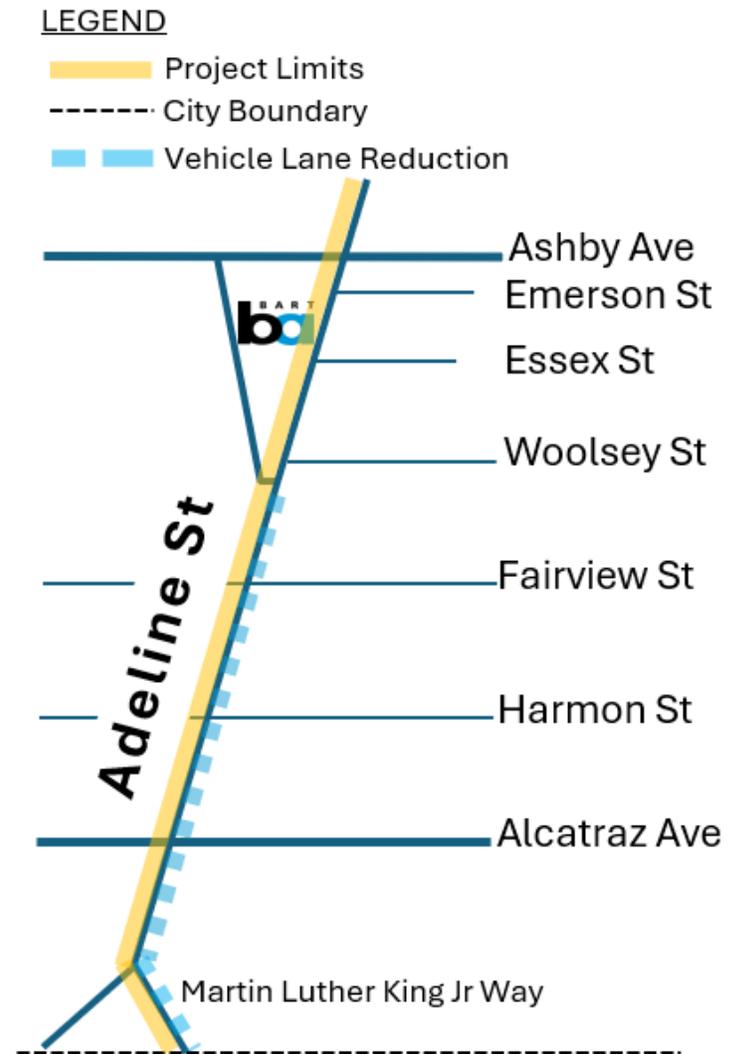


DRAFT – NOT FOR CONSTRUCTION



# Vehicle Lane Reduction

- One lane in each direction from BART to Oakland would be converted from vehicle lanes to bike lanes
- Some increased congestion likely, especially traveling south in the evening rush hour



# Safer Pedestrian Crossings



Pedestrian Refuge Island



# Bus Stops



Bus Boarding Island



## LEGEND



AC Transit Line  
F, 7, 12



Lawrence  
Berkeley Lab  
Potter St/  
JBEI Route

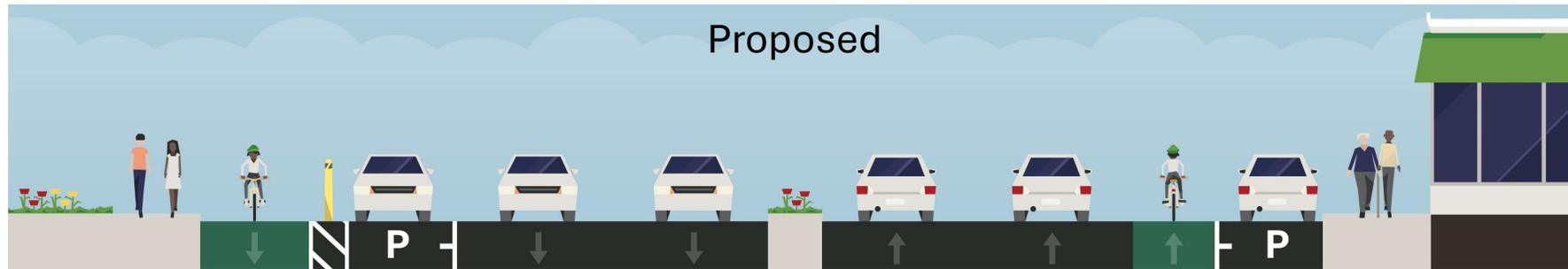
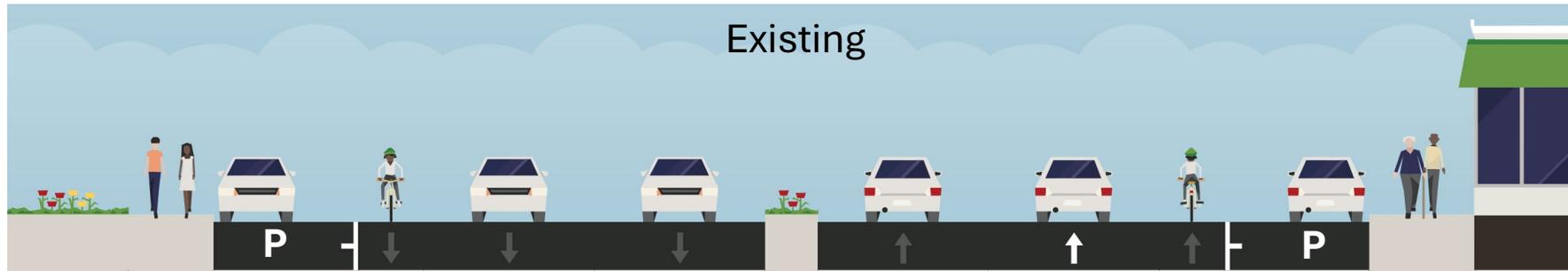


Proposed Bus  
Boarding Island



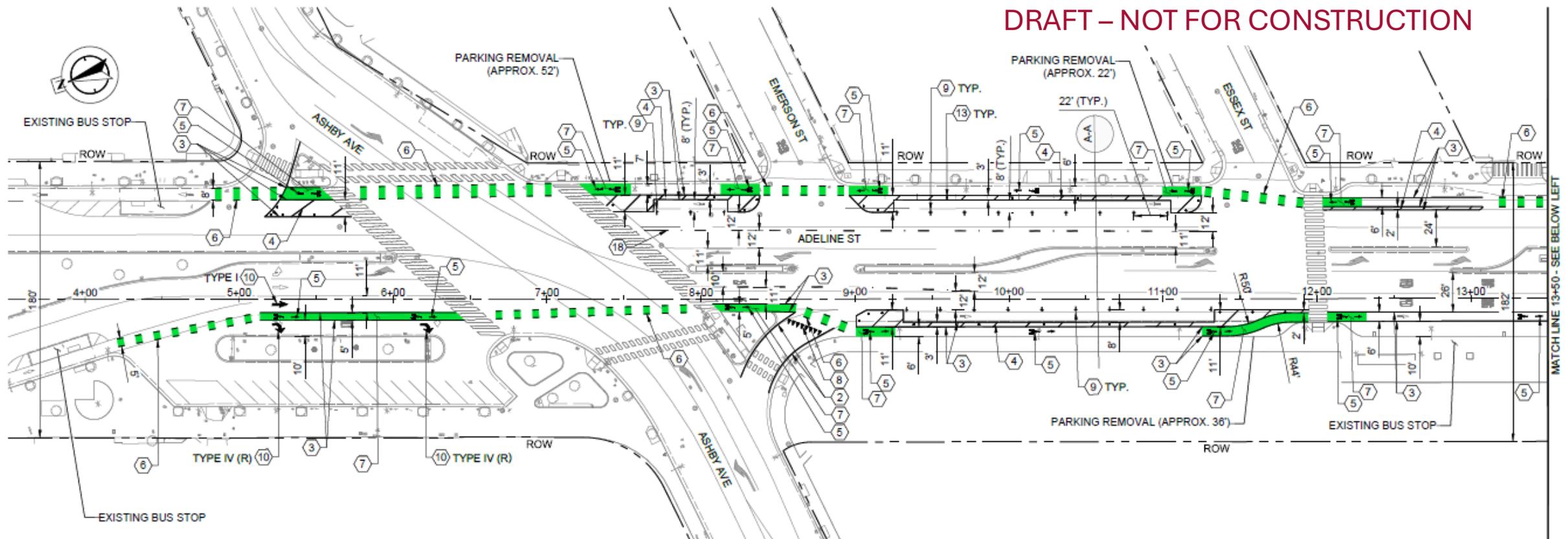
No Change

# Adeline (MLK - Ashby)





# Quick-Build Concept– Essex to Ashby



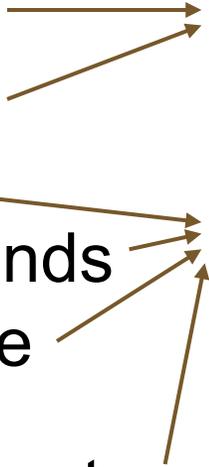
# Floating Parking



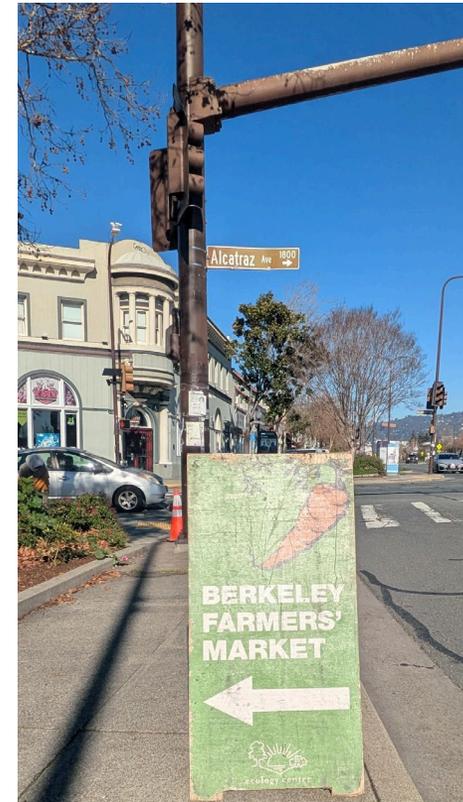
# Parking Removal



# Vision Zero Project Elements

- Removing Vehicle Lanes
  - Narrowing Vehicle Lanes
  - Adding Cycle Track
  - Adding Bus Boarding Islands
  - Adding Pedestrian Refuge Islands
  - Upgrading from Bike Lanes to Cycle Track
- Reduce Vehicle Speeds
  - Reduce Vehicle-Involved Collisions
- 

# Community Benefits



Adeline Quick Build

# Project Timeline

**Fall 2025** – Pre-Project Data Collection, Stakeholder Coordination & Public Notification

**Winter/Spring 2026** – Complete Design, Advertise Bid, and Award

**Spring 2027**– Construction

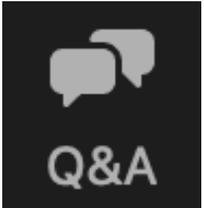
**Fall 2027**– Post-Project Evaluation to inform Adeline Transportation Improvement Project

# Questions/Comments?

## In the meeting:

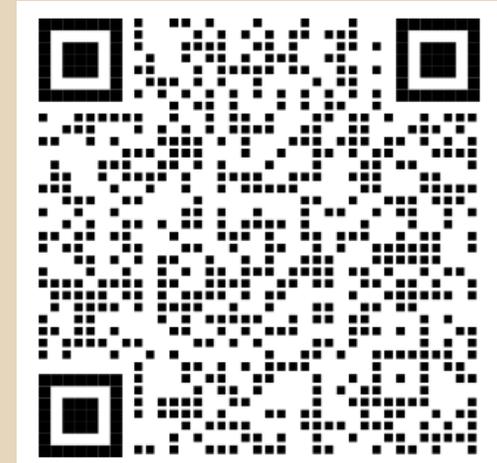


Raise your hand to be unmuted for 60 seconds.



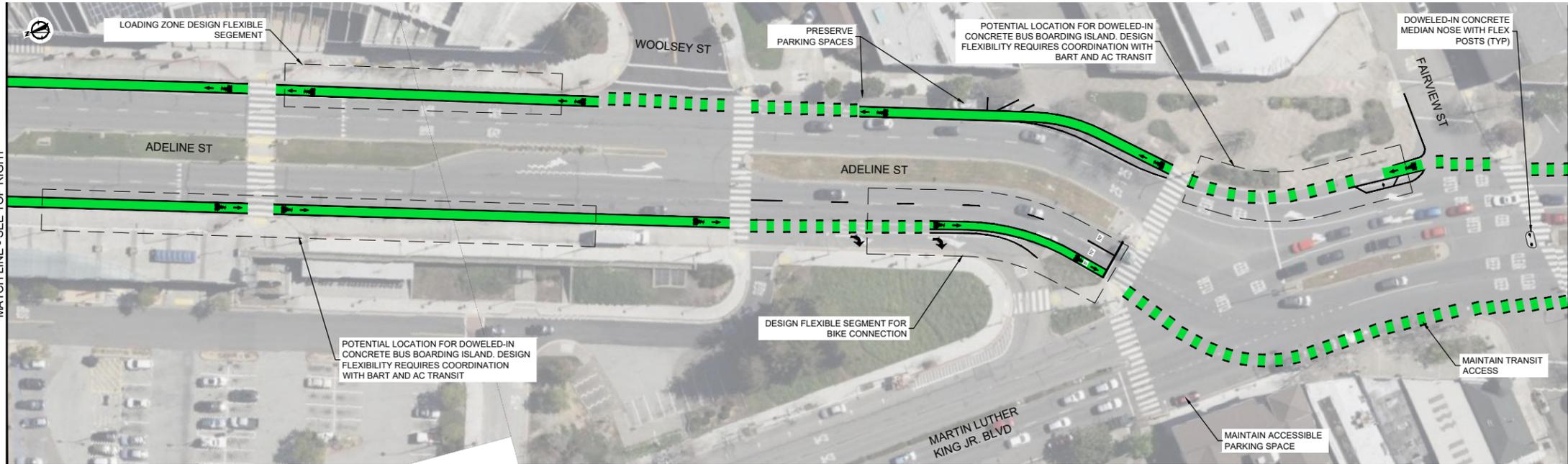
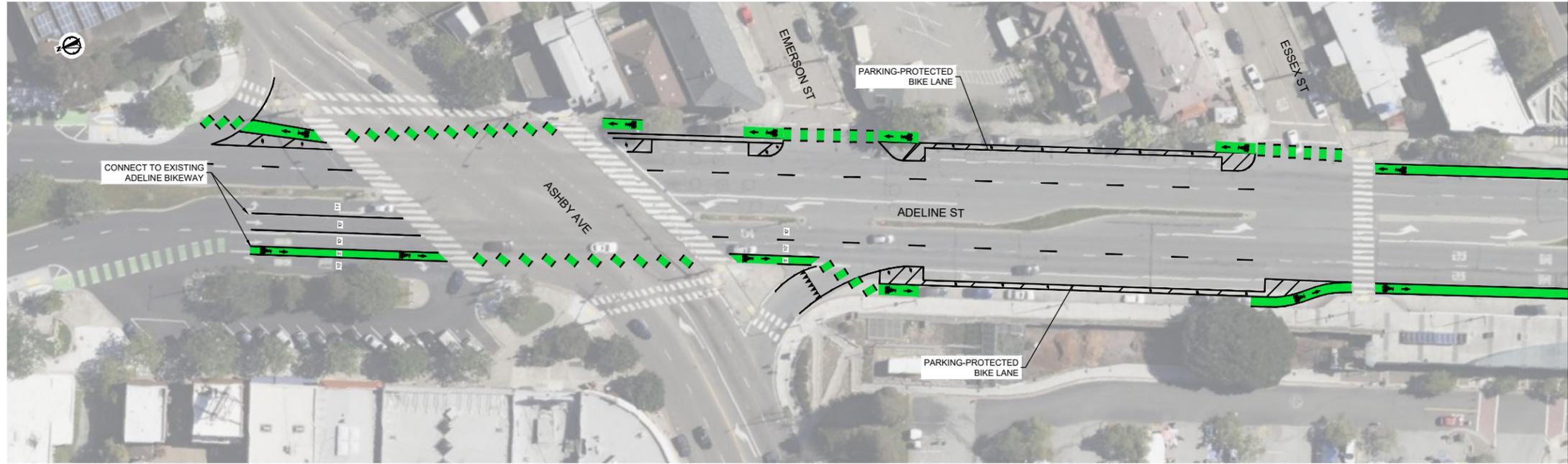
Use the Q&A Box to send to a question or comment to staff.

## Send an Email



<https://berkeleyca.gov/your-government/our-work/capital-projects/adeline-quick-build>

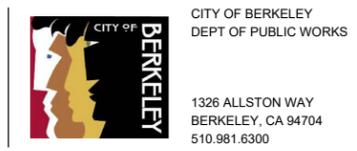
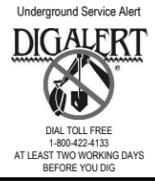
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CONCEPT PLANS  
NOT FOR CONSTRUCTION



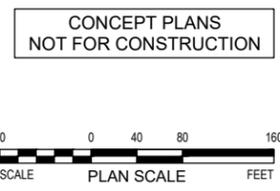
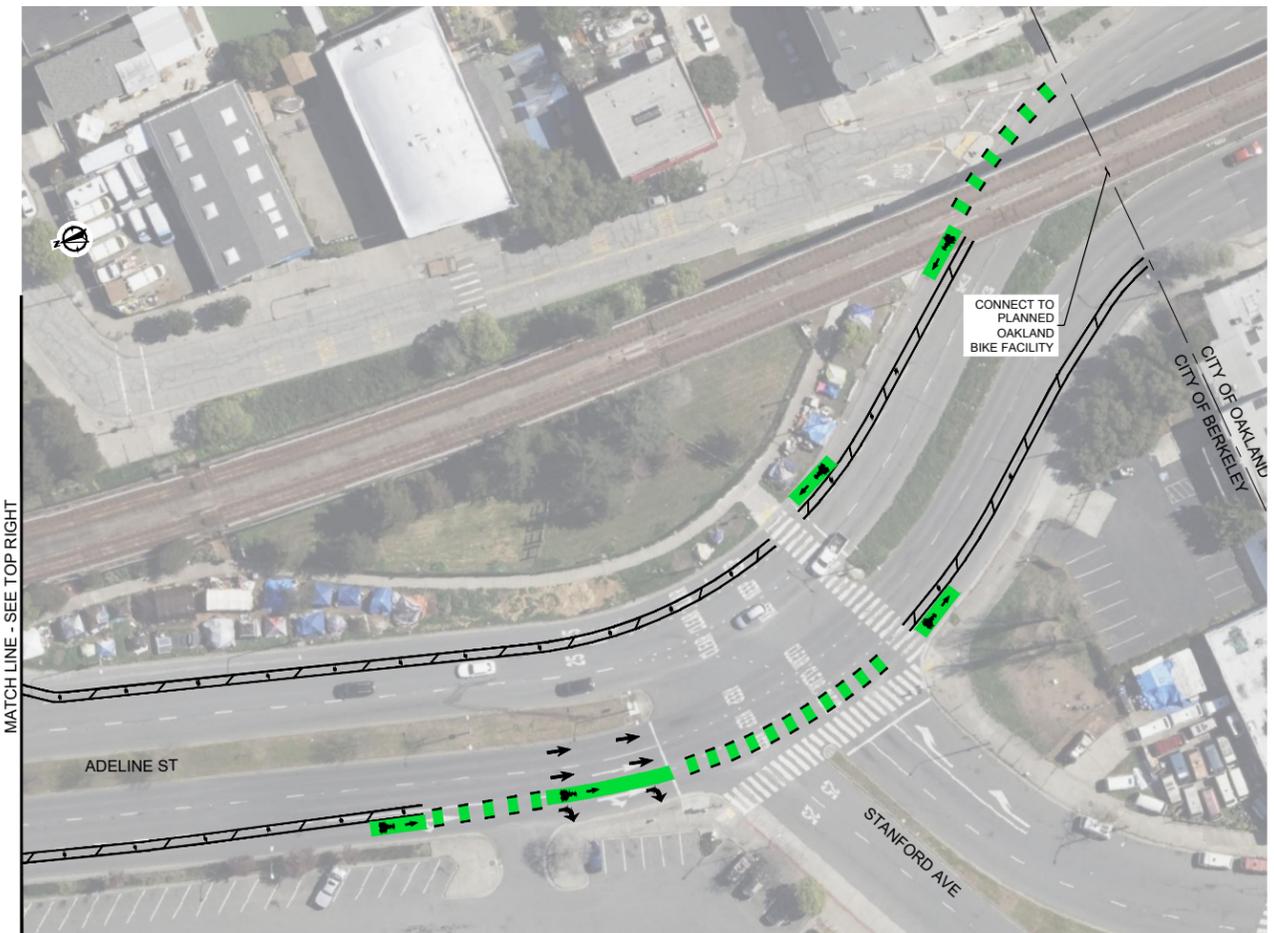
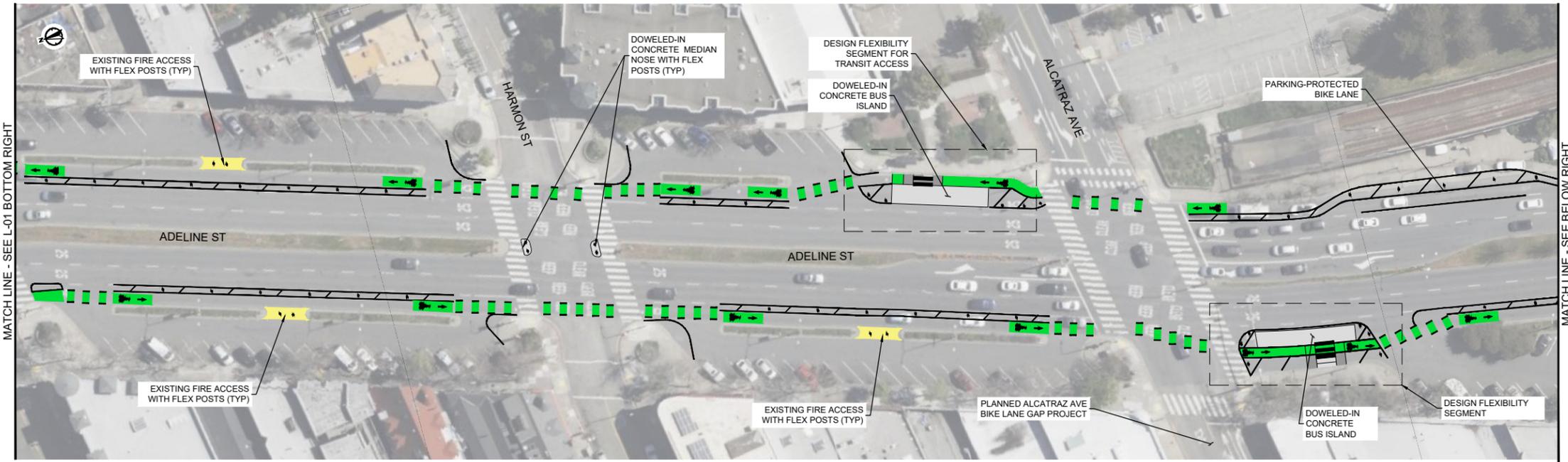
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DESIGNED BY:	MS
DRAWN BY:	MS
REVIEWED BY:	AH
DATE:	5.21.2024
SCALE:	AS SHOWN



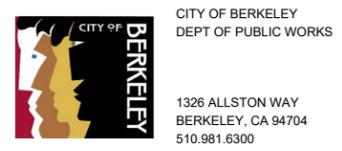
SHEET TITLE  
 CITY OF BERKELEY  
 ALAMEDA COUNTY, CALIFORNIA  
**ADELINE ST QUICK-BUILD**  
**BERKELEY ATP GRANT**  
 LAYOUT SHEET

SHEET NO.  
**L-01**  
 SHEET 1 OF 2

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PROJECT NO:	2024-056
DESIGNED BY:	MS
DRAWN BY:	MS
REVIEWED BY:	AH
DATE:	5.21.2024
SCALE:	AS SHOWN



SHEET TITLE  
 CITY OF BERKELEY  
 ALAMEDA COUNTY, CALIFORNIA  
 ADELINE ST QUICK-BUILD  
 BERKELEY ATP GRANT  
 LAYOUT SHEET

SHEET NO.  
**L-02**  
 SHEET 2 OF 2



## EL CERRITO PLAZA BART TOD

AB-2923 / SB-35 MINISTERIAL ENTITLEMENT  
 07/20/2023

REVISIONS		
#	DESCRIPTION	DATE
1	RESPONSE TO COMPLETENESS REVIEW	10/5/2023

COVER SHEET

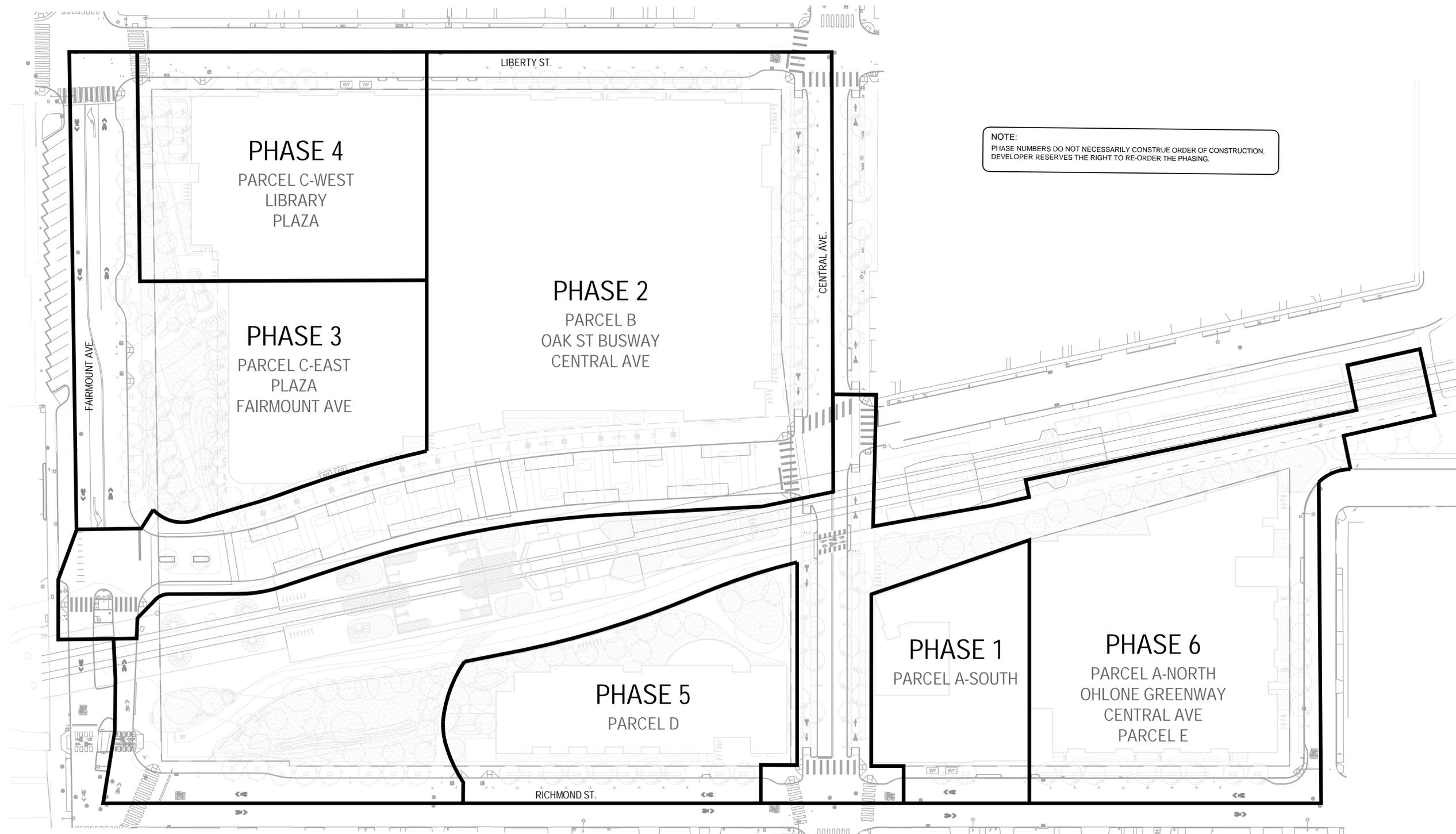
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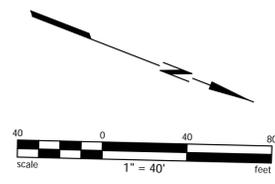
- = MARKET RATE HOUSING
- = AFFORDABLE HOUSING

AERIAL 3D VIEW - FROM NORTHEAST ①

REVISIONS		
#	DESCRIPTION	DATE
1	RESPONSE TO COMPLETENESS REVIEW	10/5/2023



NOTE:  
 PHASE NUMBERS DO NOT NECESSARILY CONSTRUCT ORDER OF CONSTRUCTION.  
 DEVELOPER RESERVES THE RIGHT TO RE-ORDER THE PHASING.



**PRELIMINARY**  
 NOT FOR CONSTRUCTION  
 DATE: 10/06/2023

BKF ENGINEERS  
 1646 N. CALIFORNIA BLVD.  
 SUITE 400  
 WALNUT CREEK, CA 94596  
 (925) 940-2200  
 www.bkf.com



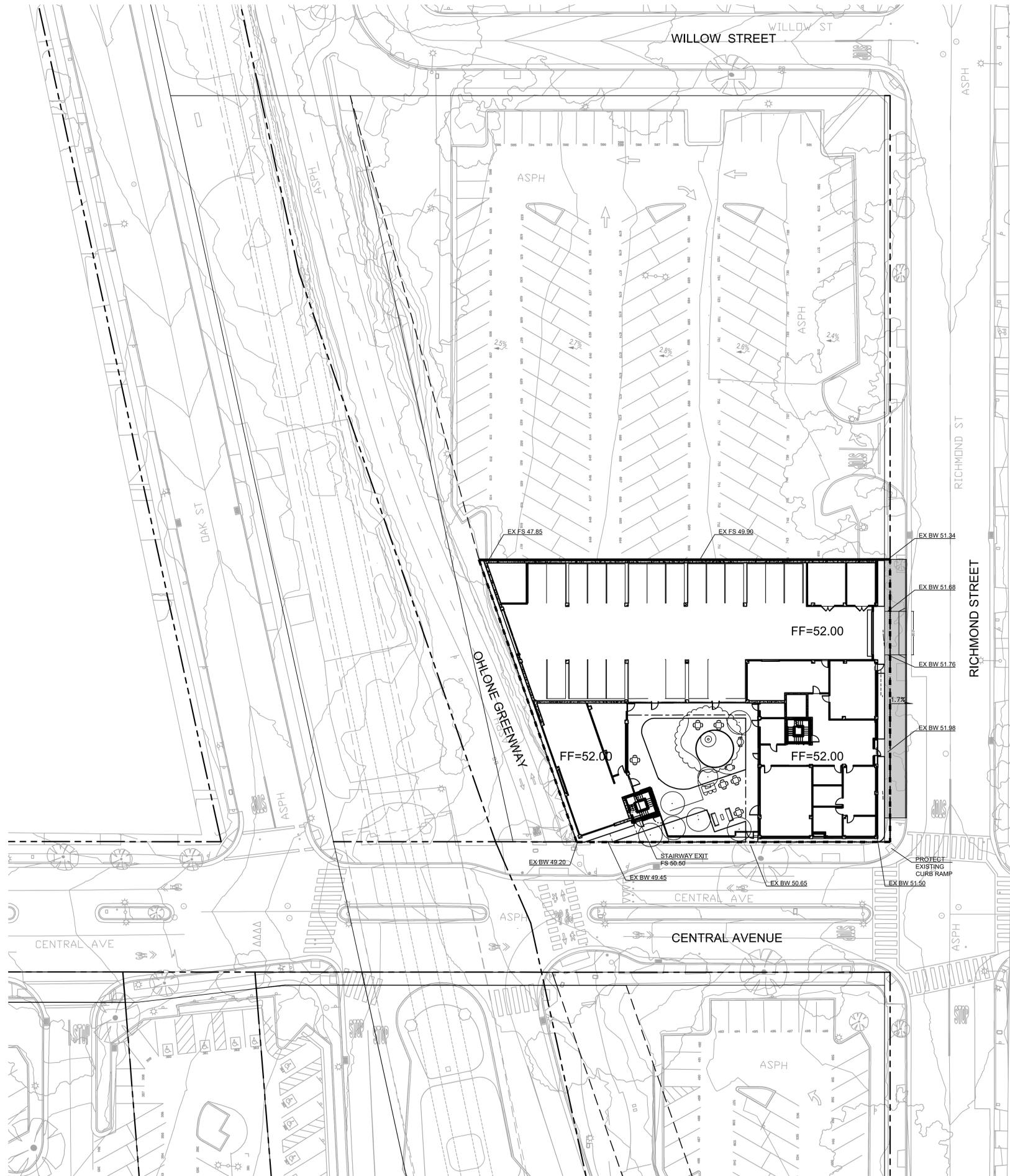
EL CERRITO PLAZA BART STATION CIVIL IMPROVEMENT  
 APN 504-130-031, 503-122-010, 504-121-017, 504-060-012  
 6671 Fairmount Ave, El Cerrito, CA 94530

PHASING PLAN

Revisions	
No.	Description
1	RESPONSE TO COMPLETENESS REVIEW 10/5/2023

Date: 07/20/2023  
 Scale: AS SHOWN  
 Design: JMM  
 Drawn: ---  
 Approved: ---  
 Job No: 20211616

Drawing Number:  
**TM-5**  
 5 OF 12



**PAVING LEGEND:**

- NEW CONCRETE SIDEWALK AND GUTTER PER CITY OF EL CERRITO SPECIFICATIONS
- ASPHALT PLUG AT CONC GUTTER
- 2" ASPHALT GRIND AND OVERLAY PER CITY OF EL CERRITO SPECIFICATIONS

**GRADING LEGEND:**

- DIRECTION OF PROPOSED DRAINAGE
- LIMIT OF GRADING
- GRADE BREAK
- DIRECTION OF EXISTING DRAINAGE
- NEW FINISH SURFACE ELEVATION
- EXISTING SURFACE ELEVATION (ES)
- TRENCH DRAIN

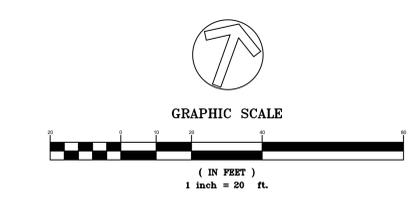
**GRADING NOTES:**

1. TC ELEVATIONS ARE 6" ABOVE ADJACENT PAVEMENT FINISH SURFACE ELEVATIONS (FS), UNLESS OTHERWISE NOTED ON PLAN.
2. NEW SIDEWALK MUST BE INSTALLED AT 2% MAX CROSS SLOPE TO MEET ACCESSIBILITY STANDARDS.

**HORIZONTAL CONTROL NOTES:**

1. ALL DIMENSIONS ARE TO FACE OF CURB UNLESS OTHERWISE NOTED.
2. CONTRACTOR SHALL MEET WITH THE CITY OF EL CERRITO'S PUBLIC WORKS AND TRAFFIC ENGINEERING DEPARTMENTS TO WORK OUT TRAFFIC PLANS THAT WILL NEED TO BE IMPLEMENTED THROUGHOUT CONSTRUCTION. APPROVAL BY BOTH DIVISIONS WILL BE REQUIRED PRIOR TO IMPLEMENTATION OF ANY TRAFFIC PLANS.
3. THE CITY TRAFFIC ENGINEER SHALL REVIEW ALL PROPOSED SIGNING AND STRIPING IN THE CITY OF EL CERRITO RIGHT OF WAY BEFORE INSTALLATION OCCURS. CONTRACTOR SHALL GIVE THE TRAFFIC ENGINEER A MINIMUM OF TWO DAYS NOTICE.
4. CONTRACTOR TO OBTAIN SEPARATE PERMIT FOR ALL WORK PERFORMED IN THE PUBLIC RIGHT OF WAY.
5. REPLACEMENT OF SIDEWALK, CURB, GUTTER AND OTHER STREETScape IMPROVEMENTS AS SHOWN ON THE GRADING PLANS SHALL COMPLY WITH CURRENT CITY OF EL CERRITO STANDARDS FOR ACCESSIBILITY.

PROPOSED PERVIOUS AND IMPERVIOUS AREAS		
NO.	ISSUE	DATE
1	EXISTING PERVIOUS AREA	1,450 SF
2	EXISTING IMPERVIOUS AREA	20,007 SF
3	TOTAL SITE AREA	21,457 SF



**ECP BART PARCEL A-SOUTH**  
 CENTRAL AVE & RICHMOND ST, EL CERRITO, CA 94530



SUBMITTAL SCHEDULE	
NO.	DATE
SB330	09.29.22
SB35	03.01.23
RESUBMITTAL	

JOB NUMBER: 2021616  
 DRAWN BY: HH  
 CHECKED BY: JNM  
 DATE: 03/01/2023  
 SCALE: AS SHOWN  
 TITLE: GRADING PLAN

SHEET:  
**C.2**

**ECP BART PARCEL A-SOUTH**  
 CENTRAL AVE. & RICHMOND ST., EL CERRITO, CA 94630

STAMP:

REVISION SCHEDULE	
NO.	DATE
1	09.29.22
2	03.01.23

JOB NUMBER:  
 DRAWN BY:  
 CHECKED BY:  
 ISSUE DATE:  
 SCALE:  
 TITLE:  
**GROUND LEVEL  
 CONCEPTUAL  
 LANDSCAPE PLAN**

SHEET:  
**L1.0**

PRELIMINARY - Not for Construction

NOTE: OFF SITE TREES ARE TO REMAIN AND BE PROTECTED IN ACCORDANCE WITH CITY APPROVED CERTIFIED ARBORIST.



4 BIKE PLAZA WITH TABLE(S) & SEATING



3 PATHWAYS AND PLANTING AREAS



6 (E) OFF SITE TREES TO REMAIN, TYP. SEE L3.0



7 NATIVE & DROUGHT TOLERANT PLANTING SEE L2.0 FOR PRELIMINARY PLANT LIST.

(4) CLASS II BIKE RACKS (8 STALLS) PER CITY STANDARDS, TO BE COORDINATED WITH MASTER PLAN STREETScape DESIGN



1 PLAY TURF AREA WITH NATURE PLAY ELEMENTS



2 CENTRAL OAK TREE WITH CIRCULAR BENCH



5 OUTDOOR LOUNGE PATIO

**ECP BART PARCEL A-SOUTH**  
 CENTRAL AVE. & RICHMOND ST., EL CERRITO, CA 94530



SUBMITTAL SCHEDULE	
NO.	ISSUE DATE
SB330	09.29.22
1 SB35	03.01.23
RESUBMITTAL	

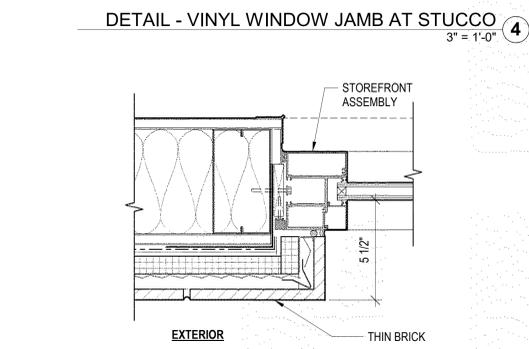
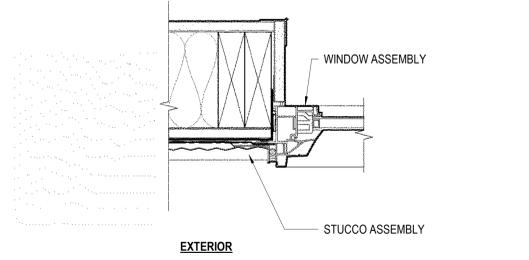
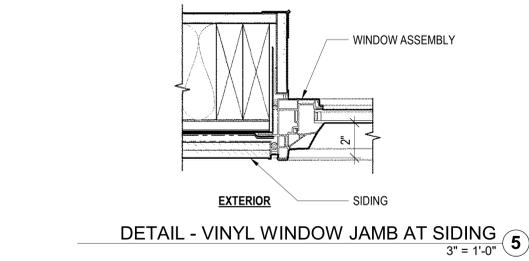
JOB NUMBER: 2211  
 DRAWN BY: CE, CS, JL  
 CHECKED BY: KM  
 DATE: 03/01/2023  
 SCALE: As indicated  
 TITLE: ELEVATIONS

SHEET:  
**A3.00**

- PRELIMINARY - Not for Construction -



**NORTH ELEVATION** ②  
 1/8" = 1'-0"



**EAST ELEVATION - RICHMOND** ①  
 1/8" = 1'-0"



**ECP BART PARCEL A-SOUTH**  
 CENTRAL AVE. & RICHMOND ST., EL CERRITO, CA 94530

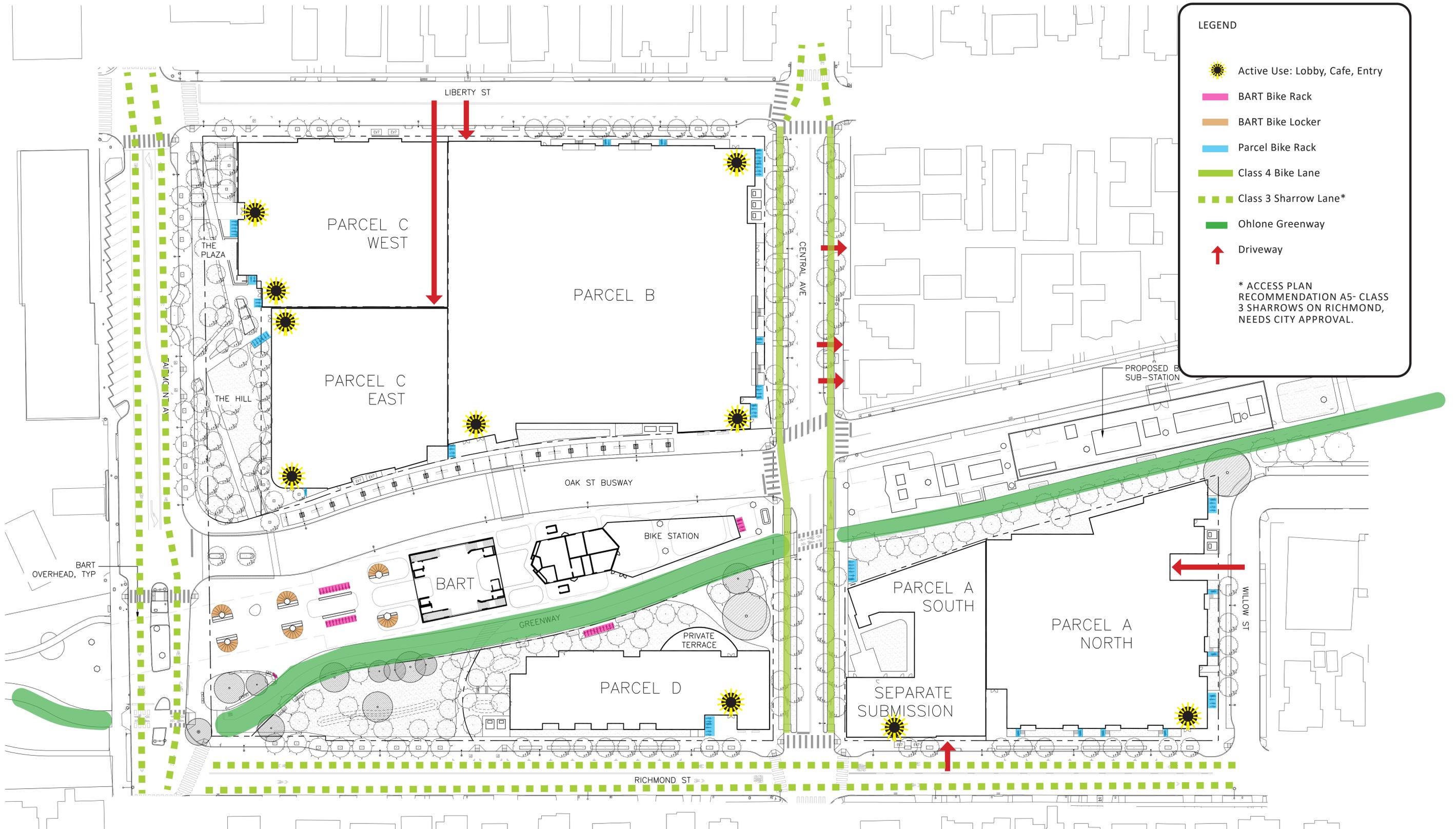


SUBMITTAL SCHEDULE

NO.	ISSUE	DATE
SB330		09.29.22
1	SB35 RESUBMITTAL	03.01.23

JOB NUMBER: 2211  
 DRAWN BY: CE, CS, JL  
 CHECKED BY: KM  
 DATE: 03/01/2023  
 SCALE: 1/8" = 1'-0"  
 TITLE:  
**ELEVATIONS**

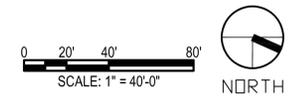
SHEET:  
**A3.01**



**LEGEND**

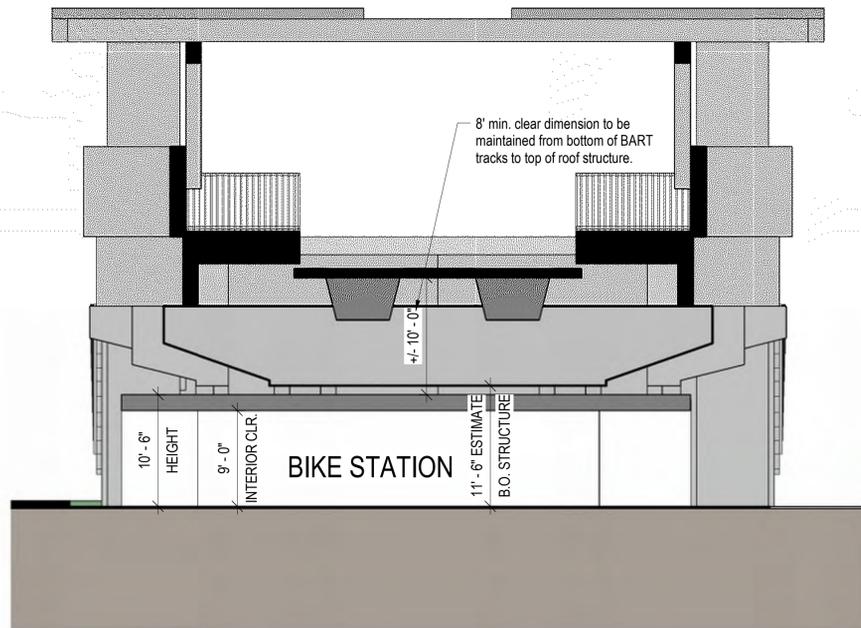
- Active Use: Lobby, Cafe, Entry
- BART Bike Rack
- BART Bike Locker
- Parcel Bike Rack
- Class 4 Bike Lane
- Class 3 Sharrows Lane\*
- Ohlone Greenway
- Driveway

\* ACCESS PLAN RECOMMENDATION A5- CLASS 3 SHARROWS ON RICHMOND, NEEDS CITY APPROVAL.

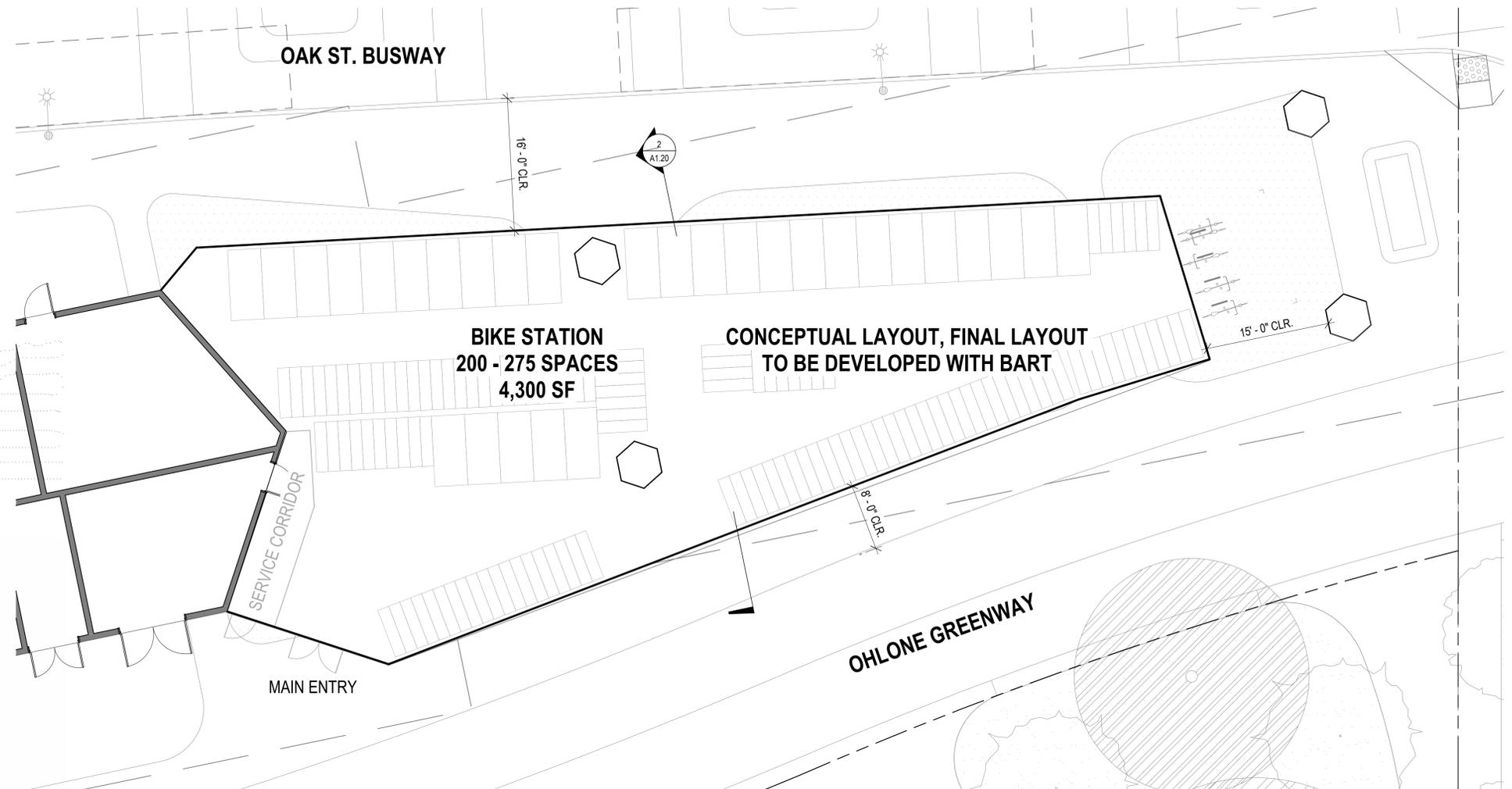


#	DESCRIPTION	DATE
1	RESPONSE TO COMPLETENESS REVIEW	10/5/2023

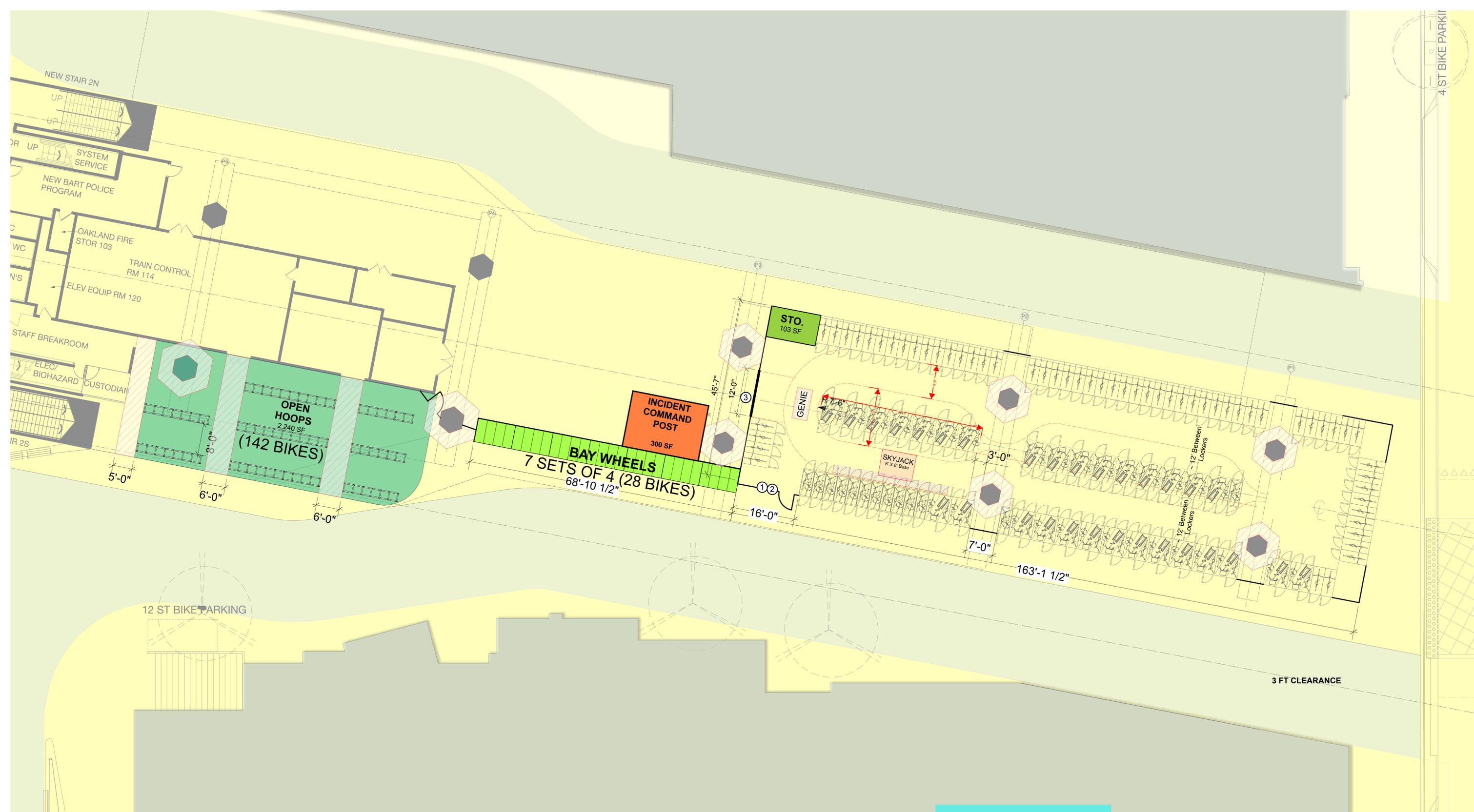




BIKE STATION SECTION 2  
1/8" = 1'-0"



BIKE STATION PLAN - NO ATTENDANT STATION 1  
1/8" = 1'-0"



## BIKE PARKING - PLAN SITE CAPACITY

**WOBART**  
OAKLAND, CA  
1/15/26

### BART CONSTRAINTS

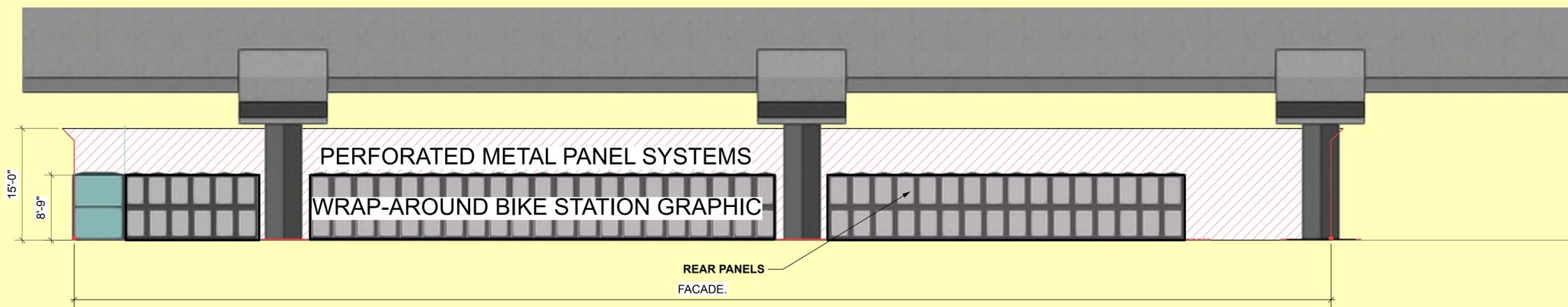
- COLUMN CLEARANCES
- OPERATION AND TURN-RADIUS - GENIE AND SKY-LARK
- ACCESS
  - ① CARD-KEY ACCESS CONTROL
  - ② MANUAL ACCESS DOORS (SWING, FOLDING, OR SLIDING)
  - ③ ROLL-UP DOORS

### DESIGN COMPONENTS

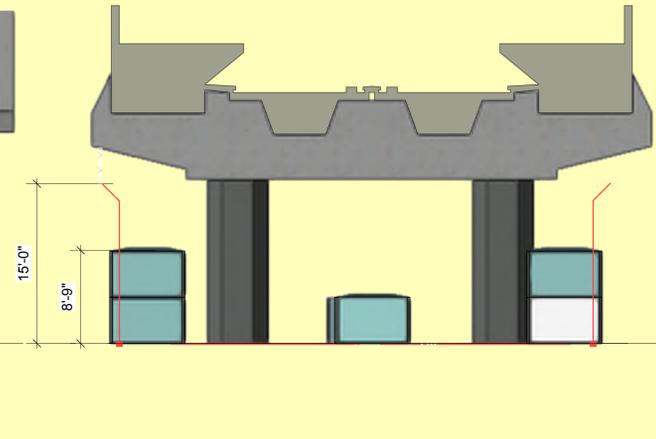
- PROPOSED MATERIAL**
- STAINLESS STEEL MODULAR FENCE ENCLOSURE
  - PERFORATED METAL PANEL SYSTEMS
  - WRAP-AROUND BIKE STATION GRAPHIC AT EXPOSED E-BIKE PANELS
  - LIGHTING (EXTERIOR & INTERIOR)
  - AIR FLOW (EXTERIOR LOUVERS & INTERIOR FANS)
- WAYFINDING**
- BIKE STATION SIGNAGE (EXTERIOR & INTERIOR)
  - SIGNAGE LIGHTING (EXTERIOR & INTERIOR)

### WOBART BIKE ACCOMMODATION

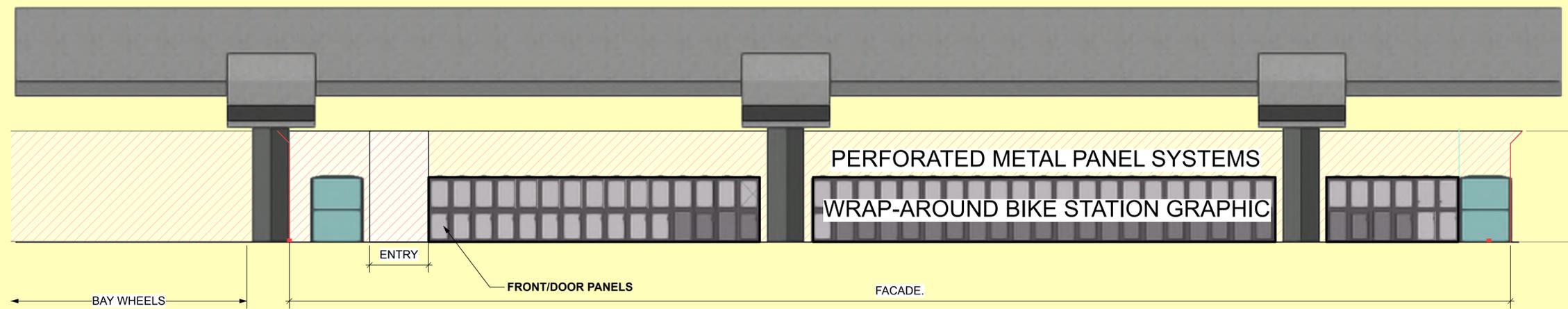
Name	Count	Bikes per Locker	Bikes
G7 eLocker_Single Tier_Double Sided_2 Bikes Each	1	2	2
G7 eLocker_Single Tier_Double Sided_3 Bikes Each	16	3	48
G7 eLocker_Two Tier_Double Sided_3 Bikes Each	12	3	36
G7 eLocker_Two Tier_Double Sided_5 Bikes Each	14	5	70
G7 eLocker_Two Tier_Single Sided_2 Bikes Each	58	2	116
<b>E-BIKES</b>			<b>272</b>
<b>GO - BIKES</b>			<b>28</b>
<b>OPEN HOOPS</b>			<b>142</b>



NORTH ELEVATION 1  
Scale: 1/8" = 1'-0"



SECTION - FLUSH PROFILE 3  
Scale: 1/8" = 1'-0"



SOUTH ELEVATION 2  
Scale: 1/8" = 1'-0"

## BIKE PARKING - SECTION

### SITE CAPACITY

WOBART  
OAKLAND, CA  
1/15/26

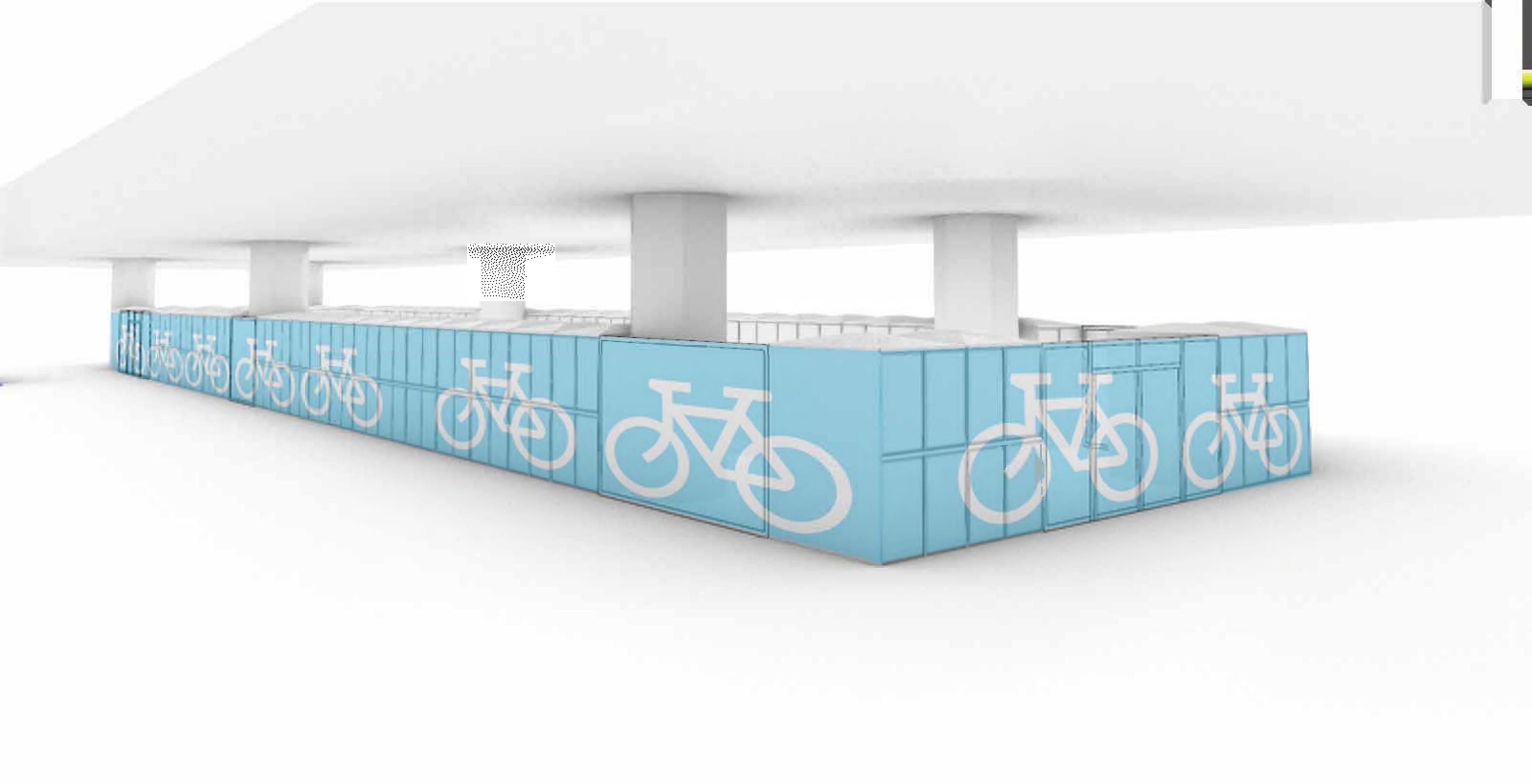
#### BART CONSTRAINTS

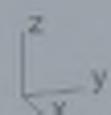
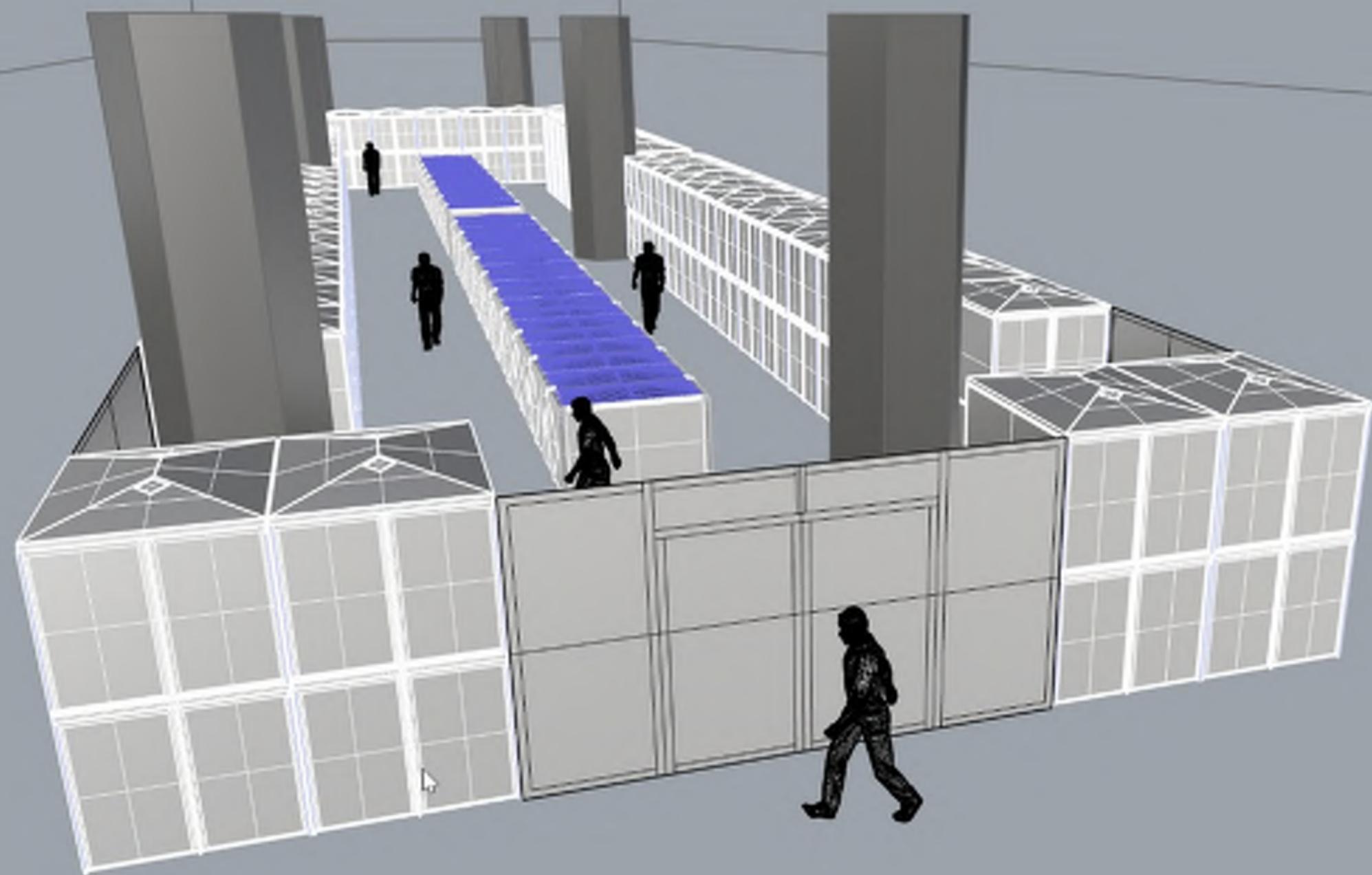
- COLUMN CLEARANCES
- OPERATION AND TURN-RADIUS - GENIE AND SKY-LARK
- ACCESS
  - 1 CARD-KEY ACCESS CONTROL
  - 2 MANUAL ACCESS DOORS (SWING, FOLDING, OR SLIDING)
  - 3 ROLL-UP DOORS

#### DESIGN COMPONENTS

- PROPOSED MATERIAL**
  - 1. GALVANIZED METAL MODULAR FENCE ENCLOSURE
  - 2. PERFORATED METAL PANEL SYSTEMS
  - 3. WRAP-AROUND BIKE STATION GRAPHIC AT EXPOSED E-BIKE PANELS
  - 4. LIGHTING (EXTERIOR & INTERIOR)
  - 5. AIR FLOW (EXTERIOR LOUVERS & INTERIOR FANS)
- WAYFINDING**
  - 1. BIKE STATION SIGNAGE (EXTERIOR & INTERIOR)
  - 2. SIGNAGE LIGHTING (EXTERIOR & INTERIOR)

#### UNIQUE FEATURE / DETAIL





# WEST OAKLAND BART STATION DEVELOPMENT

## PHASE 1 PX2500022 PERMIT SET – PUBLIC IMPROVEMENT PLANS

CITY OF OAKLAND

ALAMEDA COUNTY

STATE OF CALIFORNIA

### PROJECT DESCRIPTION

MANDELA STATION AT WEST OAKLAND BART IS A TRANSIT-ORIENTED DEVELOPMENT PROJECT ENVISIONED TO CONSIST OF MIXED COMMERCIAL AND RESIDENTIAL USES ("T3 BUILDING", "T1 BUILDING", AND "T4 BUILDING"), PUBLIC FRONTAGE IMPROVEMENTS, ENHANCED PEDESTRIAN AND BICYCLE FACILITIES, AND AN ON-SITE PUBLIC PLAZA. THE FINAL DEVELOPMENT PLAN WAS ENTITLED NOVEMBER 4, 2020 (CASEFILE PLN18490-REV02, PLN18521-REV01). THE PUBLIC STREET IMPROVEMENTS REQUIRED FOR THE DEVELOPMENT PLAN ARE TO BE PERMITTED AND CONSTRUCTED ACCORDING TO THE PHASING PLAN (SHEET C1.2).

### PHASE 1

THIS PX PHASE CONSISTS OF PUBLIC STREET IMPROVEMENTS ALONG MOST OF 5TH STREET AND CHESTER STREET. THESE IMPROVEMENTS CORRESPOND TO THE FRONTAGE FOR THE T3 AFFORDABLE HOUSING DEVELOPMENT WHICH WILL BE PERMITTED UNDER SEPARATE COVER. IMPROVEMENTS INCLUDE VEHICLE AND PEDESTRIAN ZONE ENHANCEMENT, STREET TREES, BUS AND PASSENGER LOADING ZONES, DRAINAGE IMPROVEMENTS, AND CURB RAMPS AT INTERSECTIONS AND MID-BLOCK CROSSINGS. UNDERGROUND OF FRANCHISE ELECTRICAL POWER WILL BE CONDUCTED IN PARALLEL WITH PHASE 1 PX WORK BUT DESIGNED AND DOCUMENTED AS PART OF THE T3 ON-SITE WORK. PUBLIC SANITARY SEWER EXTENSION ON 5TH STREET SIMILARLY IS TO BE PERMITTED UNDER SEPARATE COVER (PX#2500009).

### GENERAL NOTES

- SOURCE OF TOPOGRAPHY:** EXISTING TOPOGRAPHIC INFORMATION SHOWN IS BASED ON A SURVEY BY BKF ENGINEERS UNDER THE SUPERVISION OF JASON SIMMONS, PLS #9862, PERFORMED IN APRIL OF 2025. ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.
- UTILITIES:** UNDERGROUND UTILITIES PLOTTED HEREON WERE PLOTTED FROM A COMBINATION OF FIELD SURVEY, OBSERVED SURFACE EVIDENCE (CONDITIONS PERMITTING) AND RECORD INFORMATION OBTAINED FROM THE RESPECTIVE UTILITY COMPANIES, AND ARE NOT INTENDED TO REPRESENT THEIR ACTUAL LOCATIONS. THEREFORE ALL UTILITIES MUST BE VERIFIED WITH RESPECT TO SIZE, HORIZONTAL AND VERTICAL LOCATIONS BY THE OWNER AND/OR CONTRACTOR PRIOR TO DESIGN OR CONSTRUCTION. NO RESPONSIBILITY IS ASSUMED BY THE ENGINEER FOR THE LOCATION AND CAPACITY OF SAID UTILITIES.
- BOUNDARY:** THE PROPERTY BOUNDARY SHOWN HEREON IS BASED ON FIELD MEASUREMENTS AND RECORD INFORMATION AND IS WITHOUT THE BENEFIT OF A TITLE REPORT.
- BENCHMARK:** THE ELEVATIONS SHOWN HEREON ARE BASED ON A FOUND CITY OF OAKLAND BENCHMARK, DESIGNATION 25/H, BEING A BRONZE DISK STAMPED, "SEC 25 STA H." ELEVATION 10.784 FEET (CITY OF OAKLAND DATUM)
- HORIZONTAL CONTROL:** HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLANE COORDINATE SYSTEM (CCS83), EPOCH 2017.00.
- BASIS OF BEARINGS:** THE BEARING NORTH 17°06'32" EAST BETWEEN MONUMENTS ALONG 7TH STREET AT 5TH STREET AND 8TH STREET AS SHOWN ON PARCEL MAP 10662, FILED JULY 25, 2017, IN BOOK 336 OF PARCEL MAPS, AT PAGES 47 AND 48, ALAMEDA COUNTY RECORDS, WAS TAKEN AS THE BASIS OF BEARINGS FOR THIS SURVEY.
- THIS PROJECT IS EXEMPTED FROM STORMWATER TREATMENT REQUIREMENTS DUE TO ENTITLEMENTS ACCEPTANCE IN 2020.

### PROJECT DATA

**DEVELOPER:**  
KEYSTONE DEVELOPMENT GROUP  
1714 FRANKLIN STREET, 100-346  
OAKLAND, CA 94612  
PHONE: (510) 206-9130  
CONTACT: ART MAY

**LANDSCAPE ARCHITECT:**  
PGADESIGN LANDSCAPE ARCHITECTS  
444 17th STREET  
OAKLAND, CA 94612  
PHONE: (510) 845-7549  
CONTACT: MARK WESSELS

**CIVIL ENGINEER:**  
BKF ENGINEERS  
1646 N. CALIFORNIA BLVD, SUITE 400  
WALNUT CREEK, CA 94596  
PHONE: (925) 940-2200  
CONTACT: ERIC SWANSON

**TRAFFIC ENGINEER:**  
FEHRS & PEERS  
100 PRINGLE AVE  
WALNUT CREEK, CA 94596  
PHONE: (925) 930-7100  
CONTACT: ROB REES

ASSESSOR PARCEL NO.: 4-71-3  
4-77-3

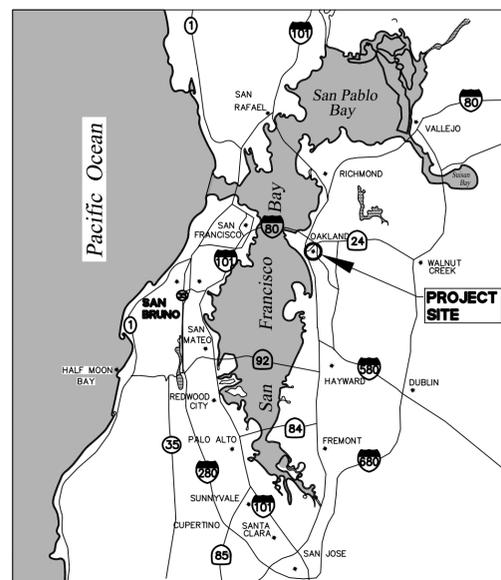
EXISTING LAND USE: COMMERCIAL

PROPOSED LAND USE: MIXED USE

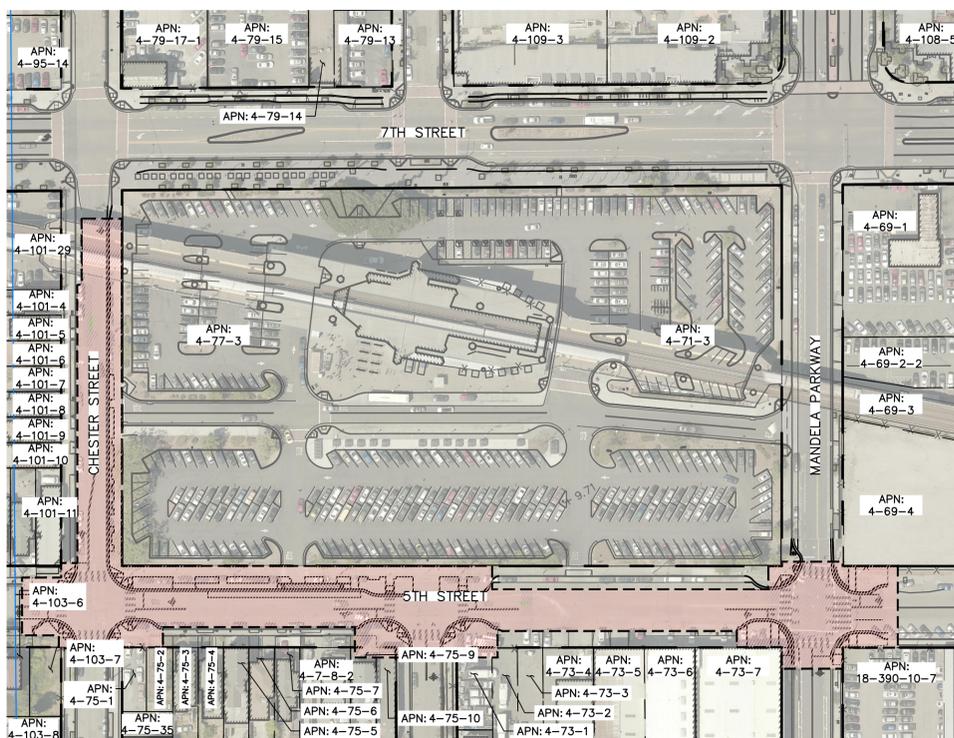
LAND AREA: 9.44 ACRES

### UTILITY INFORMATION:

WATER SUPPLY: EAST BAY MUNICIPAL UTILITY DISTRICT (EBMUD)  
FIRE PROTECTION: CITY OF OAKLAND / EBMUD  
SEWAGE DISPOSAL: CITY OF OAKLAND  
STORM DRAIN: CITY OF OAKLAND  
GAS: PACIFIC GAS & ELECTRIC (PG&E)  
ELECTRIC: PACIFIC GAS & ELECTRIC (PG&E)  
TELEPHONE: AT&T  
CABLE TELEVISION: COMCAST



LOCATION MAP NTS



SITE PLAN NTS

### ABBREVIATIONS

AD	=	AREA DRAIN
B	=	BOLLARD
BFP	=	BACKFLOW PREVENTOR
BR	=	BIKE RACK
BW	=	BACK OF WALK
CLDR	=	CENTERLINE OF DOOR
CMH	=	COMMUNICATION MANHOLE
COL	=	COLUMN
COMM	=	COMMUNICATION
CONC	=	CONCRETE
CTV	=	CABLE TELEVISION
DW	=	DRIVEWAY
E	=	EXISTING
EX	=	EXISTING
EB	=	ELECTRICAL BOX
EMH	=	ELECTRICAL MANHOLE
EP	=	EDGE OF PAVEMENT
EV	=	ELECTRICAL VAULT
FDC	=	FIRE DEPARTMENT CONNECTION
FL	=	FLOWLINE
G	=	GAS LINE
HCR	=	HANDICAP RAMP
IE	=	INVERT ELEVATION
INV	=	INVERT ELEVATION
LF	=	LINEAR FEET
LG	=	LIP OF GUTTER
MB	=	MAIL BOX
MH	=	MANHOLE
NEW	=	NEW
P	=	POST
PKM	=	PARKING METER
PR	=	PROPOSED
SD	=	STORM DRAIN
SDCO	=	STORM DRAIN CLEANOUT
SDDI	=	STORM DRAIN DROP INLET
SDMH	=	STORM DRAIN MANHOLE
SLB	=	STREET LIGHTING BOX
SSCO	=	SANITARY SEWER CLEANOUT
SLP	=	SEE LANDSCAPE PLANS
SSL	=	SIGNALS AND STREET LIGHTING
SSMH	=	SANITARY SEWER MANHOLE
TB	=	TELEPHONE BOX
TC	=	TOP OF CURB
TR	=	TREE
TSB	=	TRAFFIC SIGNAL BOX
TW	=	TOP OF WALL
UV	=	UTILITY BOX
UV	=	UTILITY VAULT
WM	=	WATER METER
WP	=	WATER PIPE

### SHEET INDEX

SHEET NO	SHEET REF	DESCRIPTION
1	C1.0	TITLE SHEET
2	C1.1	GENERAL NOTES
3	C1.2	OVERALL PHASING PLAN
4	C1.3	KEY MAP
5	C2.0	OVERALL EXISTING CONDITIONS PLAN
6	C2.1	PH-1 EXISTING CONDITIONS PLAN
7	C2.2	PH-1 EXISTING CONDITIONS PLAN
8	C2.3	PH-1 EXISTING CONDITIONS PLAN
9	C3.0	PH-1 DEMOLITION PLAN
10	C3.1	PH-1 DEMOLITION PLAN
11	C3.2	PH-1 DEMOLITION PLAN
12	C4.0	OVERALL SITE PLAN
13	C4.1	TYPICAL SECTIONS
14	C4.2	TYPICAL SECTIONS
15	C4.3	PH-1 HORIZONTAL CONTROL PLAN
16	C4.4	PH-1 HORIZONTAL CONTROL PLAN
17	C4.5	PH-1 HORIZONTAL CONTROL PLAN
18	C4.6	PH-1 SIGNAGE AND STRIPING PLAN
19	C4.7	PH-1 SIGNAGE AND STRIPING PLAN
20	C4.8	PH-1 SIGNAGE AND STRIPING PLAN
21	C5.0	PH-1 OVERALL GRADING PLAN
22	C5.1	PH-1 GRADING PLAN
23	C5.2	PH-1 GRADING PLAN
24	C5.3	PH-1 GRADING PLAN
25	C5.4	PH-1 GRADING DETAILS AND ENLARGEMENTS
26	C5.5	PH-1 GRADING DETAILS AND ENLARGEMENTS
27	C6.0	PH-1 UTILITY PLAN
28	C6.1	PH-1 UTILITY PLAN
29	C6.2	PH-1 UTILITY PLAN
30	C7.0	PH-1 PRIVATE LATERAL PROFILES
31	C8.0	CONSTRUCTION DETAILS
32	C8.1	CONSTRUCTION DETAILS
33	C8.2	CONSTRUCTION DETAILS
34	C8.3	CONSTRUCTION DETAILS
35	C8.4	CONSTRUCTION DETAILS
36	C8.5	CONSTRUCTION DETAILS
37	C8.6	CONSTRUCTION DETAILS
38	C8.7	CONSTRUCTION DETAILS
39	C8.8	CONSTRUCTION DETAILS
40	C8.9	CONSTRUCTION DETAILS
41	C8.10	CONSTRUCTION DETAILS
42	C8.11	CONSTRUCTION DETAILS
43	C8.12	CONSTRUCTION DETAILS
44	C8.13	CONSTRUCTION DETAILS
45	C8.14	CONSTRUCTION DETAILS
46	C9.0	OVERALL EROSION CONTROL PLAN
47	C9.1	EROSION CONTROL DETAILS
48	L1.0	PH-1 LANDSCAPE SITE PLAN
49	L2.1	PH-1 LANDSCAPE MATERIALS PLAN 1
50	L2.2	PH-1 LANDSCAPE MATERIALS PLAN 2
51	L2.3	PH-1 LANDSCAPE MATERIALS PLAN 3
52	L2.4	PH-1 LANDSCAPE MATERIALS PLAN 4
53	L3.1	PH-1 LANDSCAPE LAYOUT PLAN 1
54	L3.2	PH-1 LANDSCAPE LAYOUT PLAN 2
55	L3.3	PH-1 LANDSCAPE LAYOUT PLAN 3
56	L3.4	PH-1 LANDSCAPE LAYOUT PLAN 4
57	L4.0	PH-1 LANDSCAPE PLANTING SCHEDULE AND NOTES
58	L4.1	PH-1 LANDSCAPE PLANTING PLAN 1
59	L4.2	PH-1 LANDSCAPE PLANTING PLAN 2
60	L4.3	PH-1 LANDSCAPE PLANTING PLAN 3
61	L4.4	PH-1 LANDSCAPE PLANTING PLAN 4
62	L5.1	PH-1 LANDSCAPE DETAILS - PAVING
63	L5.2	PH-1 LANDSCAPE DETAILS - FURNISHINGS
64	L5.3	PH-1 LANDSCAPE DETAILS - PAVING
65	L6.0	PH-1 LANDSCAPE SCORING - PLAN
66-76	L7.1-L7.11	SPECIFICATIONS
77	I1.0	PH-1 IRRIGATION PLAN
78	I1.1	PH-1 IRRIGATION PLAN
79	I1.2	PH-1 IRRIGATION PLAN
80	I1.3	PH-1 IRRIGATION PLAN
81	I1.4	PH-1 IRRIGATION PLAN
82	LT-01	GENERAL NOTES AND LEGEND - PHASE 1
83	LT-02	LIGHTING PLAN - PHASE 1
84	LT-03	LIGHTING PLAN - PHASE 1
85	LT-04	LIGHTING PLAN - PHASE 1
86	ED-1	STANDALONE ROADWAY STREETLIGHT POLE DETAIL
87	ED-2	PEDESTRIAN STREET LIGHT POLE DETAIL
88	ED-3	PEDESTRIAN STREET LIGHT POLE DETAIL
89	TS-01	GENERAL NOTES AND LEGEND - PHASE 1
90	TS-02	GENERAL NOTES AND LEGEND - PHASE 1
91	TS-03	SIGNAL PLAN - PHASE 1
92	TS-04	SIGNAL PLAN - PHASE 1

### LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROJECT BOUNDARY
---	---	RIGHT OF WAY
---	---	CURB AND GUTTER
---	---	BACK OF WALK
---	---	CONTOUR LINE
---	---	LIMIT OF WORK
---	---	SAWCUT
---	---	STORM DRAIN LINE
---	---	SANITARY SEWER LINE
---	---	FIRE WATER LINE
---	---	DOMESTIC WATER LINE
---	---	UNKNOWN UTILITY LINE
---	---	ELECTRICAL LINE
---	---	GAS LINE
---	---	GRATE INLET
---	---	SSMH
---	---	SDMH
---	---	WATER VALVE
---	---	FIRE HYDRANT
---	---	FDC
---	---	RPBFP
---	---	PARKING LIGHT
---	---	DCDA
---	---	SANITARY SEWER CLEANOUT
---	---	POWER POLE
---	---	WATER METER
---	---	WATER VALVE
---	---	STREET LIGHT
---	---	CATV BOX
---	---	SPOT GRADE
---	---	SIGN
---	---	SIGN WITH PUSH BUTTON
---	---	SHRUB
---	---	TREE
---	---	CONCRETE PAVEMENT

### ENGINEER'S STATEMENT

THESE CONSTRUCTION DOCUMENTS HAVE BEEN PREPARED BY ME OR UNDER MY DIRECTION IN ACCORDANCE WITH STANDARD ENGINEERING PRACTICE.

*[Signature]*

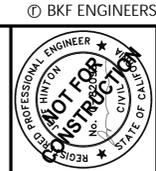
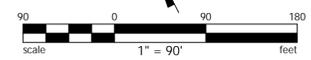
REGISTERED ENGINEER

03/16/2026

DATE



Know what's below. Call before you dig.

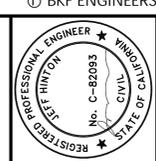


JEFF HINTON  
C-82093  
BKF ENGINEERS  
1646 N. CALIFORNIA BLVD.  
SUITE 400  
WALNUT CREEK, CA 94596  
(925) 940-2200  
WWW.BKF.COM



WEST OAKLAND BART STATION PUBLIC IMPROVEMENT  
PHASE 1 CONSTRUCTION DOCUMENTS  
CITY OF OAKLAND ALAMEDA COUNTY CALIFORNIA  
TITLE SHEET

Revisions	No.	Date	Scale	Design	Drawn	Approved	Job No.
		03/16/2026	AS SHOWN	J.M.	K.T.	J.H.	C80384



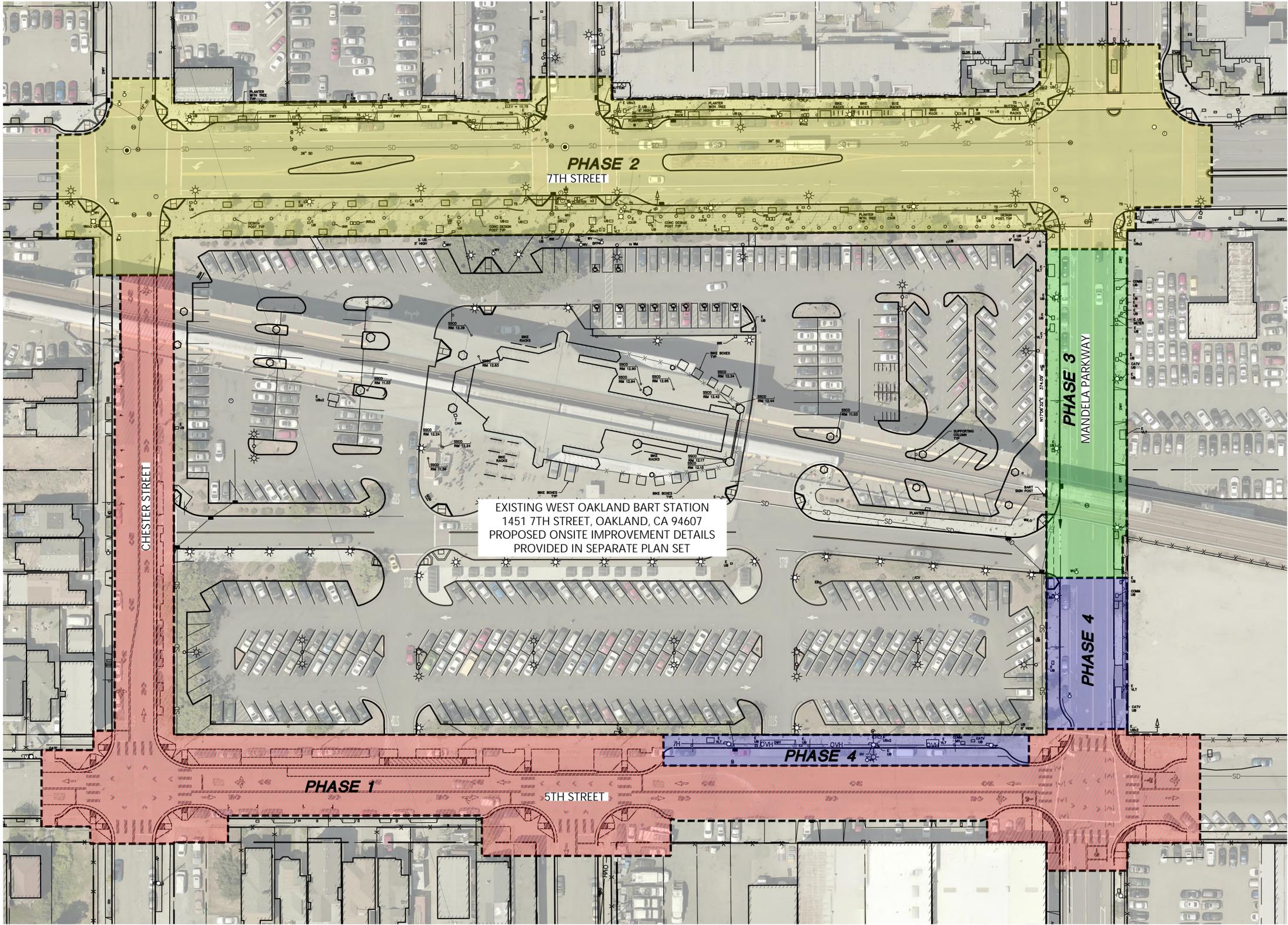
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WALNUT CREEK, CA 94596  
(925) 940-2200  
www.bkf.com



WEST OAKLAND BART STATION PUBLIC IMPROVEMENT  
PHASE 1 CONSTRUCTION DOCUMENTS  
CITY OF OAKLAND ALAMEDA COUNTY CALIFORNIA

No.	Revisions

Date: 12/22/2025  
Scale: AS SHOWN  
Design: J.M.  
Drawn: K.T.  
Approved: J.H.  
Job No: C180384

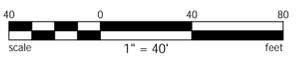


LEGEND

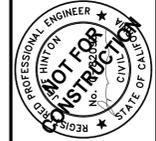
- PHASING LIMIT
- PHASE 1 IMPROVEMENT AREA
- PHASE 2 IMPROVEMENT AREA
- PHASE 3 IMPROVEMENT AREA
- PHASE 4 IMPROVEMENT AREA



Know what's below.  
Call before you dig.



PHASING PLAN



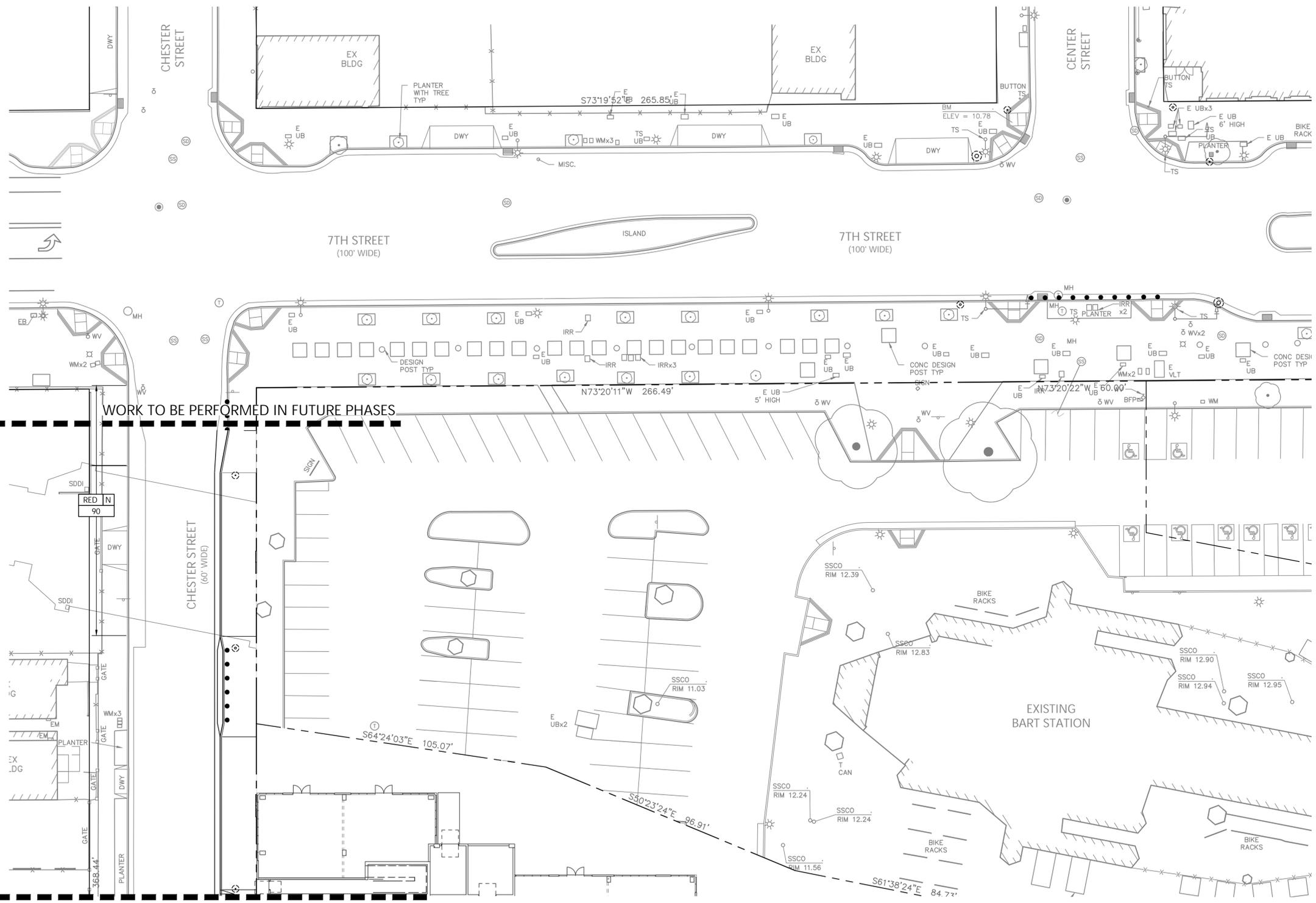
BKF ENGINEERS  
1646 N. CALIFORNIA BLVD.  
SUITE 400  
WALNUT CREEK, CA 94596  
(925) 940-2200  
WWW.BKF.COM



WEST OAKLAND BART STATION PUBLIC IMPROVEMENT  
PHASE 1 CONSTRUCTION DOCUMENTS  
CITY OF OAKLAND ALAMEDA COUNTY CALIFORNIA  
PH-1 SIGNAGE AND STRIPING PLAN

No.	Revisions

Date: 03/16/2026  
Scale: AS SHOWN  
Design: J.M.  
Drawn: K.T.  
Approved: J.H.  
Job No: C180384  
Drawing Number:  
C4.6  
18 OF 67



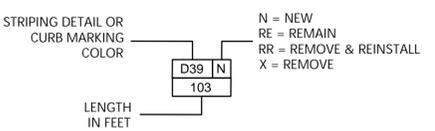
PAVEMENT MARKING LEGEND

- BICYCLE DETECTOR PAVEMENT MARKING PER DETAIL 2 SHEET C8.5
- PAVEMENT MARKING WORD 'STOP' PER CALTRANS STD PLAN A24D
- BIKE LANE EXTENSION THROUGH INTERSECTION. SEE DETAIL 2 SHEET C8.7
- SHARED LANE EXTENSION THROUGH INTERSECTION. SEE DETAIL 2 SHEET C8.8
- 8 FT TYPE IV TURN ARROW (LEFT AND RIGHT) PER CALTRANS STD PLAN A24A
- 24 FT TYPE II ARROW (LEFT AND RIGHT) PER CALTRANS STD PLAN A24B
- 24 FT TYPE III ARROW (LEFT AND RIGHT) PER CALTRANS STD PLAN A24B

SIGNING AND STRIPING NOTES

- WITHIN THE PROJECT LIMITS REMOVE AND SALVAGE ALL EXISTING SIGNAGE THAT IS IN CONFLICT WITH PROPOSED WORK (REUSE SIGN WHEN POSSIBLE). COORDINATE WITH CITY OF OAKLAND.
- EXISTING SIGNING AND STRIPING BEYOND PROJECT LIMITS ARE TO REMAIN. IF DAMAGED, CONTRACTOR SHALL RESTORE AT THEIR OWN EXPENSE.
- ALL TRAFFIC STRIPES, ARROWS, AND PAVEMENT MARKINGS SHALL BE CONSTRUCTED WITH THERMOPLASTIC MATERIAL TO THE SPECIFICATIONS SET FORTH IN SECTION 84 OF THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION (CALTRANS) STANDARD SPECIFICATIONS (LATEST EDITION)
- SIGNING AND STRIPING DETAILS REFER TO THOSE SHOWN IN THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND CALTRANS STANDARD PLANS.
- ALL EXISTING PAVEMENT MARKINGS WITHIN PROJECT LIMITS TO BE REMOVED.
- SIGN LOCATIONS TO BE CONFIRMED WITH ENGINEER PRIOR TO INSTALLATION.
- REFER TO DETAIL 4, SHEET C8.9, AND SHEETS C8.10 AND C8.11 FOR CALTRANS STANDARD DETAILS.

STRIPING AND CURB MARKING LEGEND

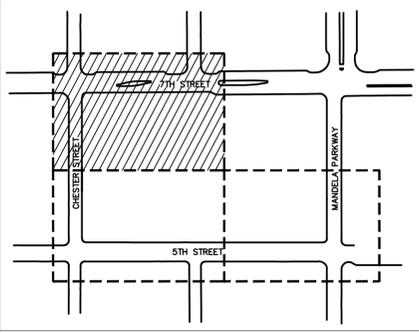
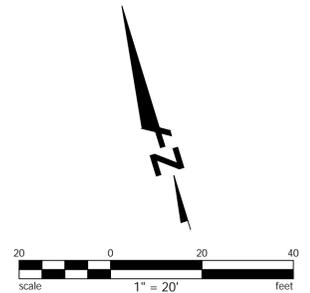


- KEYNOTES**
- RELOCATE EXISTING STOP SIGN AND POST
  - PROPOSED STOP SIGN (CA MUTCD R1-1) WITH "ALL WAY" PLAQUE (CA MUTCD R1-3P) AND POST - SEE DETAIL 1&2/C8.0
  - 4" WHITE LINES DIAGONALS AT 3' MAXIMUM O.C.
  - 'BIKES YIELD TO PEDESTRIANS' PAVEMENT MARKING. SEE DETAIL 1, SHEET C8.5.

- ABBREVIATIONS**
- |       |   |
|-------|---|
| LL    | LIMIT LINE - SEE DETAIL 1/C8.6              |
| CCW   | CONTINENTAL CROSS WALK - SEE DETAIL 4/C8.11 |
| 4"WL  | 4-INCH WHITE LINE                           |
| 8"WL  | 8-INCH WHITE LINE                           |
| 12"WL | 12-INCH WHITE LINE                          |



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KEY PLAN  
NTS





# WEST OAKLAND BART STATION DEVELOPMENT

## PHASE 2 PX2500023 PERMIT SET – PUBLIC IMPROVEMENT PLANS

CITY OF OAKLAND

ALAMEDA COUNTY

STATE OF CALIFORNIA

### PROJECT DESCRIPTION

MANDELA STATION AT WEST OAKLAND BART IS A TRANSIT-ORIENTED DEVELOPMENT PROJECT ENVISIONED TO CONSIST OF MIXED COMMERCIAL AND RESIDENTIAL USES ("T3 BUILDING", "T1 BUILDING", AND "T4 BUILDING"), PUBLIC FRONTAGE IMPROVEMENTS, ENHANCED PEDESTRIAN AND BICYCLE FACILITIES, AND AN ON-SITE PUBLIC PLAZA. THE FINAL DEVELOPMENT PLAN WAS ENTITLED NOVEMBER 4, 2020 (CASEFILE PLN18490-REV02, PLN18521-REV01). THE PUBLIC STREET IMPROVEMENTS REQUIRED FOR THE DEVELOPMENT PLAN ARE TO BE PERMITTED AND CONSTRUCTED ACCORDING TO THE PHASING PLAN (SHEET C1.2).

### PHASE 2

THIS PX PHASE CONSISTS OF PUBLIC STREET IMPROVEMENTS ALONG 7TH STREET FROM CHESTER STREET, TO MANDELA WAY. IMPROVEMENTS INCLUDE VEHICLE AND PEDESTRIAN ZONE ENHANCEMENT, STREET TREES, BUS STOPS, BUS AND PASSENGER LOADING ZONES, DRAINAGE IMPROVEMENTS, AND CURB RAMPS AT INTERSECTIONS AND MID-BLOCK CROSSINGS.

### GENERAL NOTES

- SOURCE OF TOPOGRAPHY:** EXISTING TOPOGRAPHIC INFORMATION SHOWN IS BASED ON A SURVEY BY BKF ENGINEERS UNDER THE SUPERVISION OF JASON SIMMONS, PLS #9862, PERFORMED IN APRIL OF 2025. ALL DISTANCES AND DIMENSIONS ARE IN FEET AND DECIMALS THEREOF.
- UTILITIES:** UNDERGROUND UTILITIES PLOTTED HEREON WERE PLOTTED FROM A COMBINATION OF FIELD SURVEY, OBSERVED SURFACE EVIDENCE (CONDITIONS PERMITTING) AND RECORD INFORMATION OBTAINED FROM THE RESPECTIVE UTILITY COMPANIES, AND ARE NOT INTENDED TO REPRESENT THEIR ACTUAL LOCATIONS. THEREFORE ALL UTILITIES MUST BE VERIFIED WITH RESPECT TO SIZE, HORIZONTAL AND VERTICAL LOCATIONS BY THE OWNER AND/OR CONTRACTOR PRIOR TO DESIGN OR CONSTRUCTION. NO RESPONSIBILITY IS ASSUMED BY THE ENGINEER FOR THE LOCATION AND CAPACITY OF SAID UTILITIES.
- BOUNDARY:** THE PROPERTY BOUNDARY SHOWN HEREON IS BASED ON FIELD MEASUREMENTS AND RECORD INFORMATION AND IS WITHOUT THE BENEFIT OF A TITLE REPORT.
- BENCHMARK:** THE ELEVATIONS SHOWN HEREON ARE BASED ON A FOUND CITY OF OAKLAND BENCHMARK, DESIGNATION 25/H, BEING A BRONZE DISK STAMPED, "SEC 25 STA H." ELEVATION 10.784 FEET (CITY OF OAKLAND DATUM)
- HORIZONTAL CONTROL:** HORIZONTAL COORDINATES ARE BASED OFF OF CALIFORNIA STATE PLANE COORDINATE SYSTEM (CCS83), EPOCH 2017.00.
- BASIS OF BEARINGS:** THE BEARING NORTH 17°06'32" EAST BETWEEN MONUMENTS ALONG 7TH STREET AT 5TH STREET AND 8TH STREET AS SHOWN ON PARCEL MAP 10662, FILED JULY 25, 2017, IN BOOK 336 OF PARCEL MAPS, AT PAGES 47 AND 48, ALAMEDA COUNTY RECORDS, WAS TAKEN AS THE BASIS OF BEARINGS FOR THIS SURVEY.
- THIS PROJECT IS EXEMPTED FROM STORMWATER TREATMENT REQUIREMENTS DUE TO ENTITLEMENTS ACCEPTANCE IN 2020.

### PROJECT DATA

**DEVELOPER:**  
KEYSTONE DEVELOPMENT GROUP  
1714 FRANKLIN STREET, 100-346  
OAKLAND, CA 94612  
PHONE: (510) 206-9130  
CONTACT: ART MAY

**LANDSCAPE ARCHITECT:**  
PGADDESIGN LANDSCAPE ARCHITECTS  
444 17th STREET  
OAKLAND, CA 94612  
PHONE: (510) 845-7549  
CONTACT: MARK WESSELS

**CIVIL ENGINEER:**  
BKF ENGINEERS  
1646 N. CALIFORNIA BLVD, SUITE 400  
WALNUT CREEK, CA 94596  
PHONE: (925) 940-2200  
CONTACT: ERIC SWANSON

**TRAFFIC ENGINEER:**  
FEHRS & PEERS  
100 PRINGLE AVE  
WALNUT CREEK, CA 94596  
PHONE: (925) 930-7100  
CONTACT: ROB REES

**ASSESSOR PARCEL NO.:** 4-71-3  
4-77-3

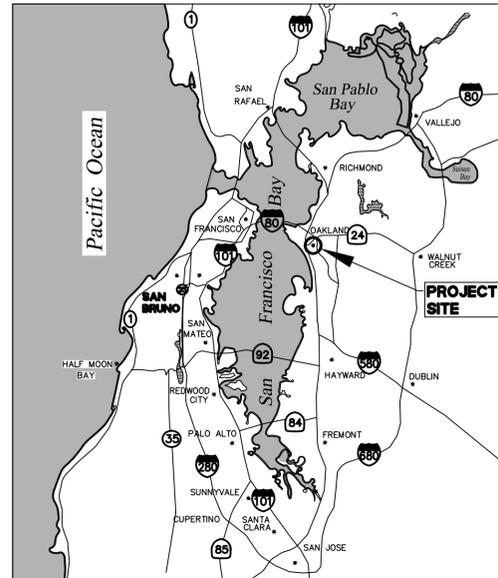
**EXISTING LAND USE:** COMMERCIAL

**PROPOSED LAND USE:** MIXED USE

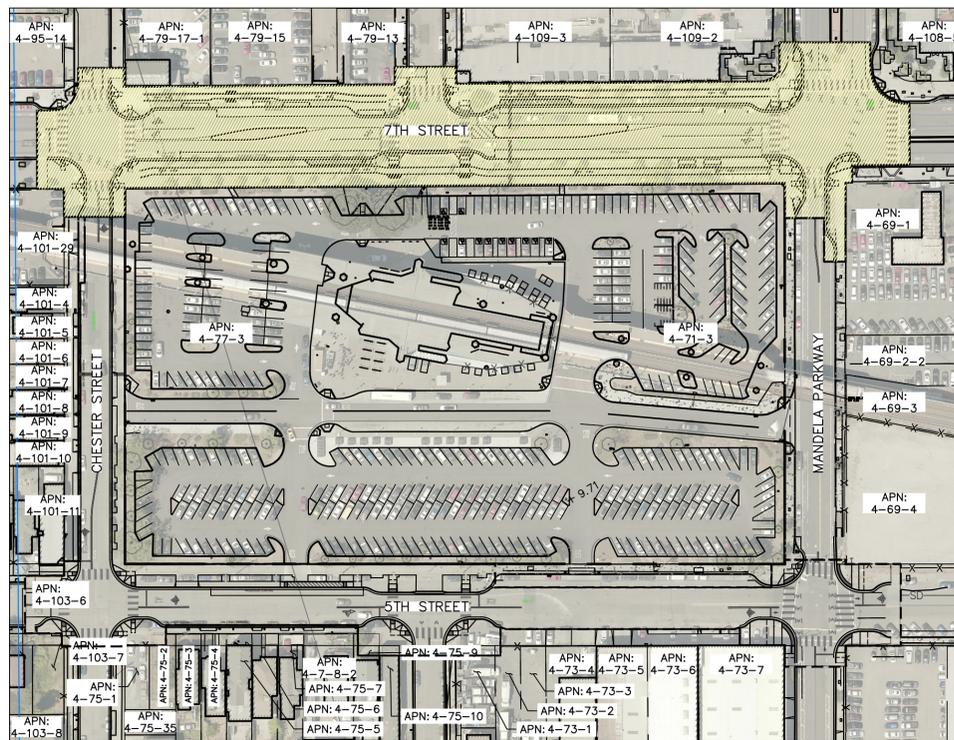
**LAND AREA:** 9.44 ACRES

**UTILITY INFORMATION:**

WATER SUPPLY: EAST BAY MUNICIPAL UTILITY DISTRICT (EBMUD)  
FIRE PROTECTION: CITY OF OAKLAND / EBMUD  
SEWAGE DISPOSAL: CITY OF OAKLAND  
STORM DRAIN: CITY OF OAKLAND  
GAS: PACIFIC GAS & ELECTRIC (PG&E)  
ELECTRIC: PACIFIC GAS & ELECTRIC (PG&E)  
TELEPHONE: AT&T  
CABLE TELEVISION: COMCAST



LOCATION MAP NTS



SITE PLAN NTS

### ABBREVIATIONS

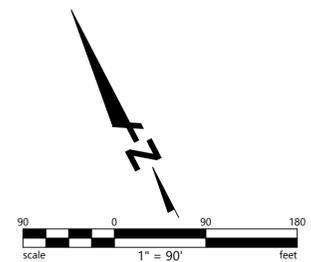
AD	=	AREA DRAIN
B	=	BOLLARD
BFP	=	BACKFLOW PREVENTOR
BR	=	BIKE RACK
BW	=	BACK OF WALK
CLDR	=	CENTERLINE OF DOOR
CMH	=	COMMUNICATION MANHOLE
COL	=	COLUMN
COMM	=	COMMUNICATION
CONC	=	CONCRETE
CTV	=	CABLE TELEVISION
DW	=	DRIVEWAY
E	=	EXISTING
EX	=	EXISTING
EB	=	ELECTRICAL BOX
EMH	=	ELECTRICAL MANHOLE
EP	=	EDGE OF PAVEMENT
EY	=	ELECTRICAL VAULT
FDC	=	FIRE DEPARTMENT CONNECTION
FL	=	FLOWLINE
G	=	GAS LINE
HCR	=	HANDICAP RAMP
IE	=	INVERT ELEVATION
INV	=	INVERT ELEVATION
LF	=	LINEAR FEET
LG	=	LIP OF GUTTER
MB	=	MAIL BOX
MH	=	MANHOLE
NEW	=	NEW
P	=	POST
PKM	=	PARKING METER
PR	=	PROPOSED
SD	=	STORM DRAIN
SDCO	=	STORM DRAIN CLEANOUT
SDDI	=	STORM DRAIN DROP INLET
SDMH	=	STORM DRAIN MANHOLE
SLB	=	STREET LIGHTING BOX
SSCO	=	SANITARY SEWER CLEANOUT
SLP	=	SEE LANDSCAPE PLANS
SSL	=	SIGNALS AND STREET LIGHTING
SSMH	=	SANITARY SEWER MANHOLE
TB	=	TELEPHONE BOX
TC	=	TOP OF CURB
TR	=	TREE
TSB	=	TRAFFIC SIGNAL BOX
TW	=	TOP OF WALL
UB	=	UTILITY BOX
UV	=	UTILITY VAULT
WM	=	WATER METER
WP	=	WATER PIPE

### SHEET INDEX

SHEET NO	SHEET REF	DESCRIPTION
1	C1.0	TITLE SHEET
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3	C1.2	OVERALL PHASING PLAN
4	C1.3	KEY MAP
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6	C2.1	PH-2 EXISTING CONDITIONS PLAN
7	C2.2	PH-2 EXISTING CONDITIONS PLAN
8	C3.0	PH-2 DEMOLITION PLAN
9	C3.1	PH-2 DEMOLITION PLAN
10	C4.0	OVERALL SITE PLAN
11	C4.1	TYPICAL SECTIONS
12	C4.2	PH-2 HORIZONTAL CONTROL PLAN
13	C4.3	PH-2 HORIZONTAL CONTROL PLAN
14	C4.4	PH-2 SIGNAGE AND STRIPING PLAN
15	C4.5	PH-2 SIGNAGE AND STRIPING PLAN
16	C5.0	PH-2 OVERALL GRADING PLAN
17	C5.1	PH-2 GRADING PLAN
18	C5.2	PH-2 GRADING PLAN
19	C5.3	PH-2 GRADING DETAILS AND ENLARGEMENTS
20	C5.4	PH-2 GRADING DETAILS AND ENLARGEMENTS
21	C6.0	PH-2 UTILITY PLAN
22	C6.1	PH-2 UTILITY PLAN
23	C7.0	PH-2 PRIVATE LATERAL PROFILES
24	C8.0	CONSTRUCTION DETAILS
25	C8.1	CONSTRUCTION DETAILS
26	C8.2	CONSTRUCTION DETAILS
27	C8.3	CONSTRUCTION DETAILS
28	C8.4	CONSTRUCTION DETAILS
29	C8.5	CONSTRUCTION DETAILS
30	C8.6	CONSTRUCTION DETAILS
31	C8.7	CONSTRUCTION DETAILS
32	C8.8	CONSTRUCTION DETAILS
33	C8.9	CONSTRUCTION DETAILS
34	C8.10	CONSTRUCTION DETAILS
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57	L6.0	PH-2 LANDSCAPE SCORING PLAN ENLARGEMENT
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70	I1.2	PH-2 IRRIGATION PLAN 2
71	I1.3	PH-2 IRRIGATION PLAN 3
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74	LT-02	LIGHTING PLAN - PHASE 2
75	LT-03	LIGHTING PLAN - PHASE 2
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81	TS-04	SIGNAL PLAN - PHASE 2
82	LT-05	SIGNAL PLAN - PHASE 2
83	LT-06	SIGNAL PLAN - PHASE 2

### LEGEND

EXISTING	PROPOSED	DESCRIPTION
---	---	PROJECT BOUNDARY
---	---	RIGHT OF WAY
---	---	CURB AND GUTTER
---	---	CONTOUR LINE
---	---	LIMIT OF WORK
---	---	SAWCUT
---	---	STORM DRAIN LINE
---	---	SANITARY SEWER LINE
---	---	FIRE WATER LINE
---	---	DOMESTIC WATER LINE
---	---	UNKNOWN UTILITY LINE
---	---	ELECTRICAL LINE
---	---	GAS LINE
---	---	GRATE INLET
---	---	SSMH
---	---	SDMH
---	---	WATER VALVE
---	---	FIRE HYDRANT
---	---	FDC
---	---	RPBFP
---	---	PARKING LIGHT
---	---	DCCA
---	---	SANITARY SEWER CLEANOUT
---	---	POWER POLE
---	---	WATER METER
---	---	WATER VALVE
---	---	STREET LIGHT
---	---	CATV BOX
---	---	SPOT GRADE
---	---	SIGN
---	---	SIGN WITH PUSH BUTTON
---	---	SHRUB
---	---	TREE
---	---	CONCRETE PAVEMENT



### ENGINEER'S STATEMENT

THESE CONSTRUCTION DOCUMENTS HAVE BEEN PREPARED BY ME OR UNDER MY DIRECTION IN ACCORDANCE WITH STANDARD ENGINEERING PRACTICE.

\_\_\_\_\_  
REGISTERED ENGINEER

\_\_\_\_\_  
DATE 03/13/2026

C-82093

JEFF HINTON

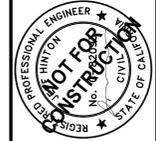
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WEST OAKLAND BART STATION PUBLIC IMPROVEMENT  
PHASE 1 CONSTRUCTION DOCUMENTS  
CITY OF OAKLAND ALAMEDA COUNTY CALIFORNIA

TITLE SHEET

Date: 03/23/2026	Revisions
Scale: AS SHOWN	No.
Design: J.M.	
Drawn: K.T.	
Approved: J.H.	
Job No: C180384	

**C1.0**  
1 OF 60



C-82093

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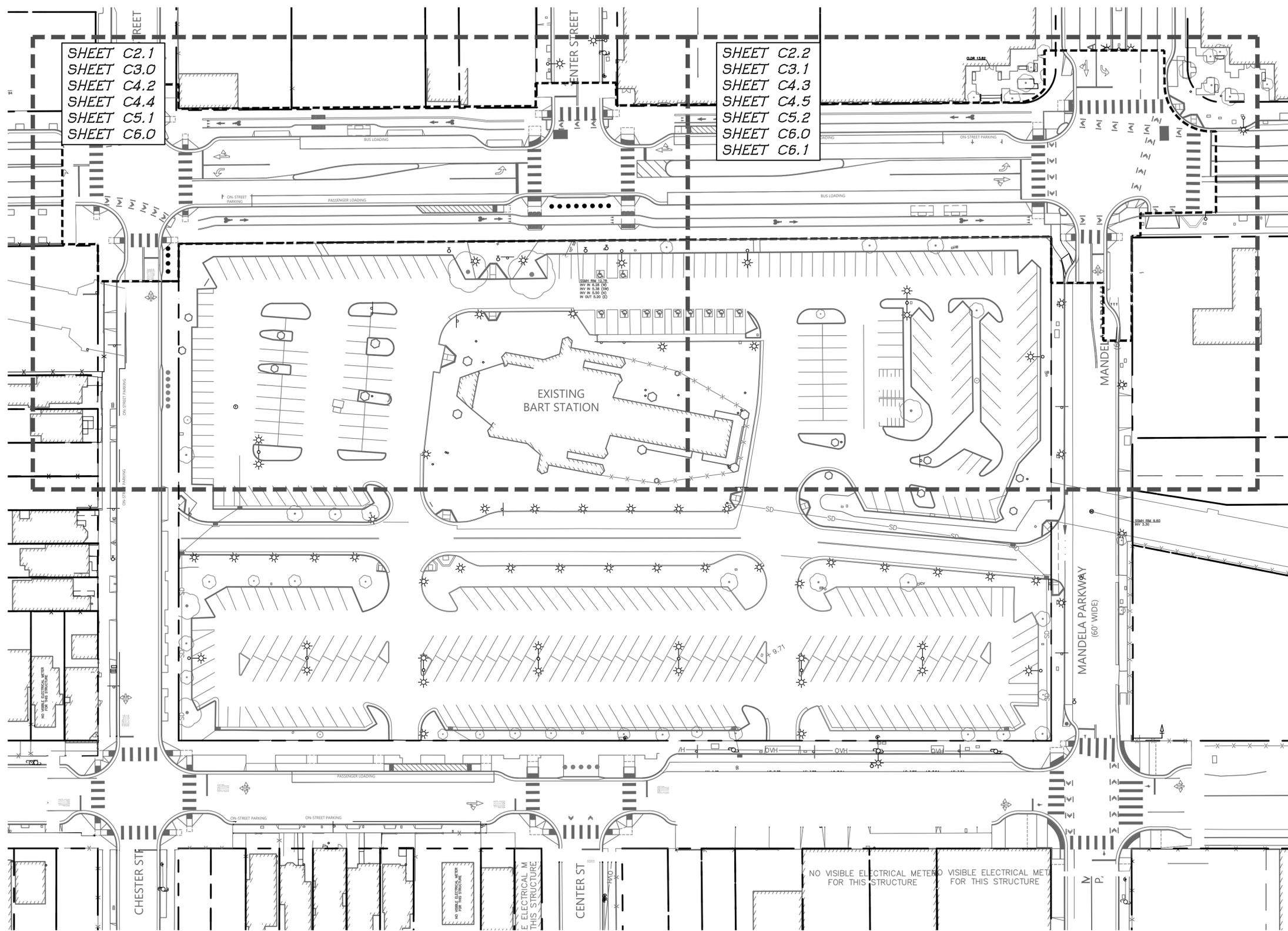


**WEST OAKLAND BART STATION PUBLIC IMPROVEMENT**  
PHASE 1 CONSTRUCTION DOCUMENTS  
CITY OF OAKLAND ALAMEDA COUNTY CALIFORNIA

**KEY MAP**

Revisions	No.	Date: 03/23/2026	Scale: AS SHOWN	Design: J.M.	Drawn: K.T.	Approved: J.H.	Job No: C180384

Drawing Number:  
**C1.4**  
4 OF 60



SHEET C2.1  
SHEET C3.0  
SHEET C4.2  
SHEET C4.4  
SHEET C5.1  
SHEET C6.0

SHEET C2.2  
SHEET C3.1  
SHEET C4.3  
SHEET C4.5  
SHEET C5.2  
SHEET C6.0  
SHEET C6.1

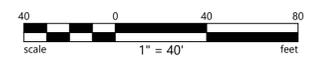
**LEGEND**

- PROPERTY LINE
- - - PHASE 2 LIMIT OF WORK
- VIEWPORT EXTENTS

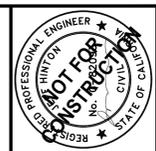
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83	LT-06	SIGNAL PLAN - PHASE 2

811  
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Plot: Mar 23, 2026 at 11:42am C:\2026\BKF\West\_Oakland\_Bart\_Station\_Survey\BKF\Sheet23\_offsite\_improvement\Phase 200\_WOBART\_OI\_CS\_P12.dwg



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 C-82093



**WEST OAKLAND BART STATION PUBLIC IMPROVEMENT**  
 PHASE 1 CONSTRUCTION DOCUMENTS  
 CITY OF OAKLAND ALAMEDA COUNTY CALIFORNIA

**TYPICAL SECTIONS**

No.	Revisions

Date: 03/23/2026  
 Scale: AS SHOWN  
 Design: J.M.  
 Drawn: K.T.  
 Approved: J.H.  
 Job No: C180384

**ABBREVIATIONS:**

- AC ASPHALT CONCRETE
- BB BIKE BUFFER
- BOC BACK OF CURB
- BL BIKE LANE
- BW BACK OF WALK
- BOW BOTTOM OF WALL
- CL CENTER LINE
- DWY DRIVEWAY
- EXIST EXISTING
- FC FACE OF CURB
- FL FLOW LINE
- HMA HOT MIX ASPHALT
- O.C. ON CENTER
- P/L PROPERTY LINE
- R/W RIGHT OF WAY
- S.E.P. SEE ELECTRICAL PLANS
- SHLD SHOULDER
- S.L.P. SEE LANDSCAPE PLANS
- S.S.P. SEE STRUCTURAL PLANS
- SW SIDEWALK
- TC TOP OF CURB

**LEGEND:**

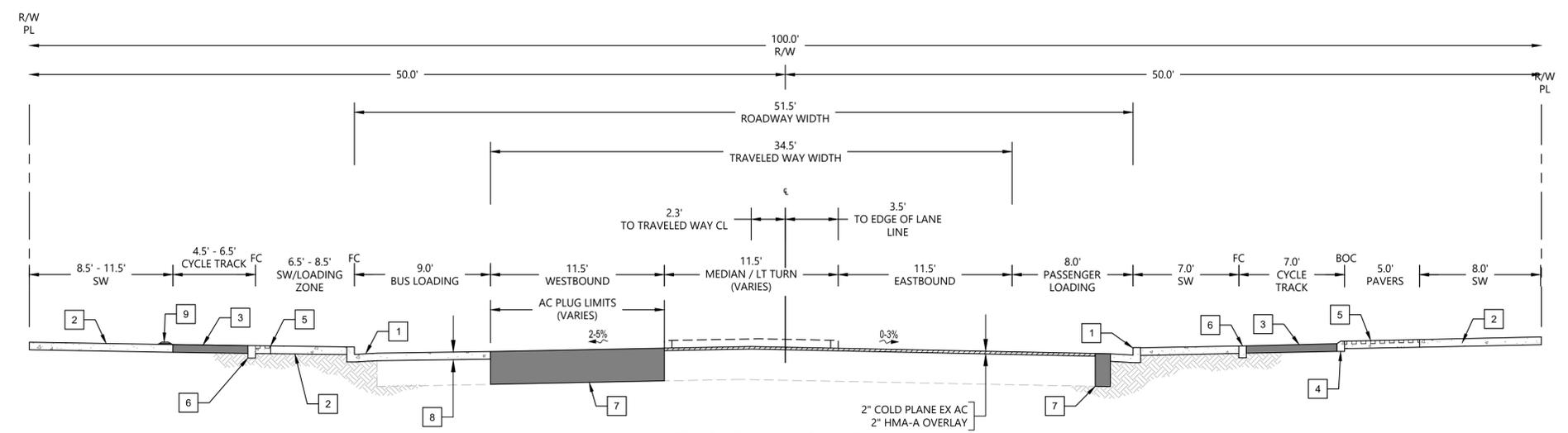
- MILL AND FILL AC
- PLAIN CEMENT CONCRETE
- FULL DEPTH AC
- EXISTING AC
- EXISTING CONCRETE
- CENTER LINE
- PROPERTY LINE

**KEYNOTES:**

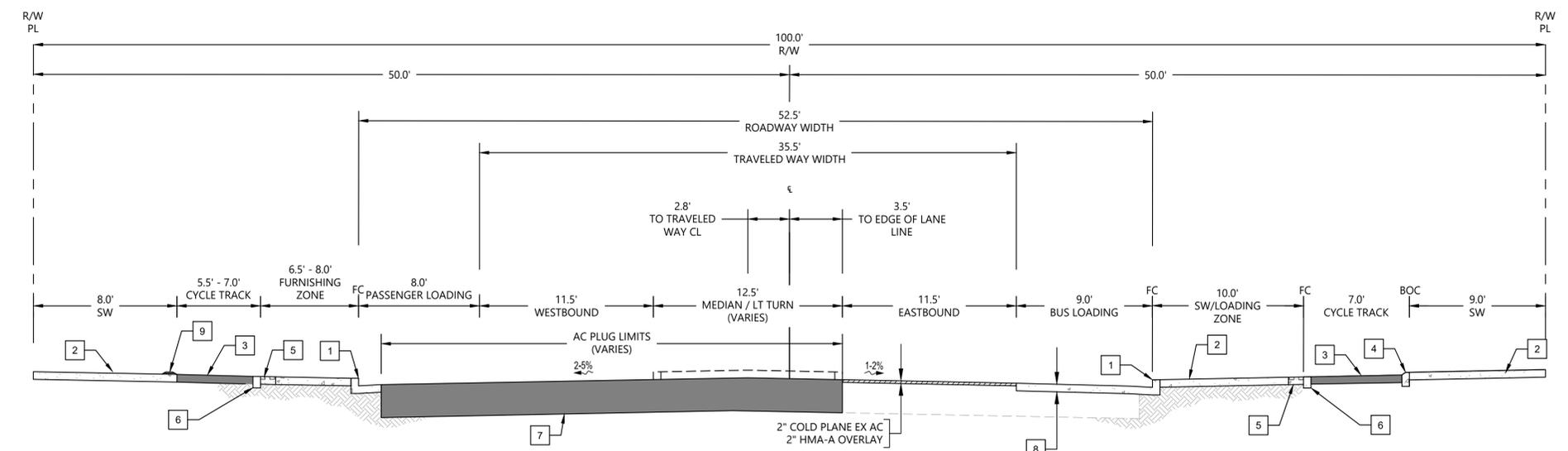
- 1 TYPE A CURB AND GUTTER PER CITY OF OAKLAND DETAIL S-1 - SEE DETAIL 1, SHEET C8.1
- 2 STANDARD SIDEWALK - SEE LANDSCAPE PLANS
- 3 AC CYCLE TRACK - SEE DETAIL 8, SHEET C8.13
- 4 SLANTED, 2" VERTICAL CURB - SEE DETAIL 2, SHEET C8.13
- 5 CONCRETE PAVERS - FOR DETECTABLE WARNING (1" MIN. WIDTH) - SEE LANDSCAPE PLANS
- 6 FLUSH CURB - SEE DETAIL 1, SHEET C8.13
- 7 FULL DEPTH ASPHALT CONCRETE PLUG - SEE DETAIL 6, SHEET C8.13
- 8 CONCRETE BUS PAD - SEE DETAIL 5, SHEET C8.13
- 9 TRAPEZOIDAL WARNING DELINEATORS - TEKWAY OR APPROVED EQUAL

**NOTES:**

- 1. DIMENSIONS AND LOCATIONS ARE TYPICAL. FOR ACTUAL DIMENSIONS AND LOCATIONS, SEE THE HORIZONTAL CONTROL PLANS.
- 2. DIMENSIONS OF THE STRUCTURAL SECTIONS ARE SUBJECT TO TOLERANCES SPECIFIED IN THE STANDARD SPECIFICATIONS.
- 3. FOR LIMITS AND TYPES OF CURB AND GUTTER, LIMIT OF DIKES, LIMITS OF SIDEWALK, AND LIMITS OF FENCING, SEE HORIZONTAL CONTROL PLANS AND DETAILS.
- 4. FOR DETAILS OF CURB AND GUTTER, AND SIDEWALK, SEE THE CONSTRUCTION DETAIL PLANS
- 5. WIDTH MAY VARY DEPENDING ON THE TYPE OF ADJACENT LOADING ZONE AND LANDSCAPE AREA CONSTRAINTS. MINIMUM PASSENGER LOADING ZONE WIDTH TO BE 8 FEET. MINIMUM BUS LOADING ZONE AREA TO BE 10 FEET.
- 6. EXISTING ROAD AND GUTTER FLOW SLOPES ARE FLATTER IN SOME AREAS THAN INDUSTRY ACCEPTED DESIGN MINIMUMS. THIS DESIGN DOES NOT SET OUT TO REMEDIATE EXISTING PONDING AS GRADES ARE CONSTRAINED BY THE WORK LIMITS AND CONFORMING CONDITIONS. THIS WORK MAY RESULT IN CHANGES TO DEPTH AND DURATION OF PONDING AT LOCALIZED POSITIONS WITH NO NET ADVERSE IMPACTS ON A BLOCKWIDE BASIS.

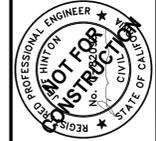


**7TH STREET - MINOR ARTERIAL**  
 STATION 20+62 TO STATION 22+46  
 DESIGN SPEED = 25 MPH  
 \*SECTION VARIES WITHIN STATED STATION INTERVAL  
 NO SCALE



**7TH STREET - MINOR ARTERIAL**  
 STATION 23+99 TO STATION 26+43  
 DESIGN SPEED = 25 MPH  
 \*SECTION VARIES WITHIN STATED STATION INTERVAL  
 NO SCALE

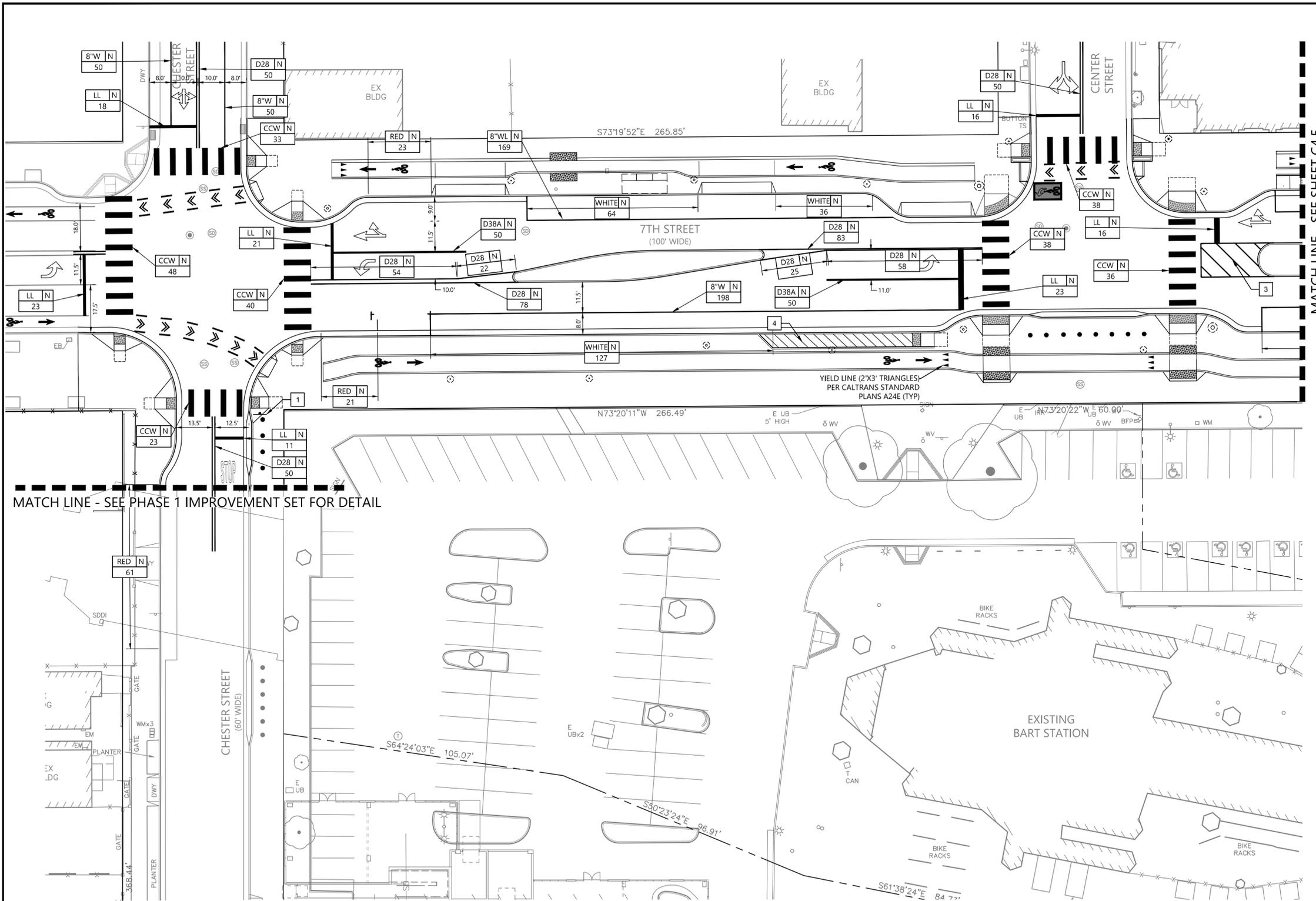
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 Date: 03/23/2026 at 11:57am  
 User: jhinton



BKF ENGINEERS  
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SUITE 400  
WALNUT CREEK, CA 94596  
(925) 940-2200  
www.bkf.com  
JEFF HINTON  
C-82093



**WEST OAKLAND BART STATION PUBLIC IMPROVEMENT**  
PHASE 1 CONSTRUCTION DOCUMENTS  
CITY OF OAKLAND ALAMEDA COUNTY CALIFORNIA  
**PH-2 SIGNAGE AND STRIPING PLAN**



**PAVEMENT MARKING LEGEND**

- BIKE LANE SYMBOL, AND 6FT ARROW PER DETAIL 3 SHEET C8.4
- PAVEMENT MARKING WORD 'STOP', 'BUS', 'ONLY' PER CALTRANS STD PLAN A24D/E
- BIKE LANE EXTENSION THROUGH INTERSECTION. SEE DETAIL 2 SHEET C8.7
- 8 FT TYPE IV TURN ARROW (LEFT AND RIGHT) PER CALTRANS STD PLAN A24A
- 24 FT TYPE II ARROW (LEFT AND RIGHT) PER CALTRANS STD PLAN A24B
- 24 FT TYPE III ARROW (LEFT AND RIGHT) PER CALTRANS STD PLAN A24B

**SIGNING AND STRIPING NOTES**

- WITHIN THE PROJECT LIMITS REMOVE AND SALVAGE ALL EXISTING SIGNAGE THAT IS IN CONFLICT WITH PROPOSED WORK (REUSE SIGN WHEN POSSIBLE). COORDINATE WITH CITY OF OAKLAND.
- EXISTING SIGNING AND STRIPING BEYOND PROJECT LIMITS ARE TO REMAIN. IF DAMAGED, CONTRACTOR SHALL RESTORE AT THEIR OWN EXPENSE.
- ALL TRAFFIC STRIPES, ARROWS, AND PAVEMENT MARKINGS SHALL BE CONSTRUCTED WITH THERMOPLASTIC MATERIAL TO THE SPECIFICATIONS SET FORTH IN SECTION 84 OF THE STATE OF CALIFORNIA DEPARTMENT OF TRANSPORTATION (CALTRANS) STANDARD SPECIFICATIONS (LATEST EDITION)
- SIGNING AND STRIPING DETAILS REFER TO THOSE SHOWN IN THE CALIFORNIA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND CALTRANS STANDARD PLANS.
- ALL EXISTING PAVEMENT MARKINGS WITHIN PROJECT LIMITS TO BE REMOVED.
- SIGN LOCATIONS TO BE CONFIRMED WITH ENGINEER PRIOR TO INSTALLATION.
- REFER TO SHEET C9.2 - C9.4 FOR CALTRANS STANDARD DETAILS.

**STRIPING AND CURB MARKING LEGEND**

- STRIPING DETAIL OR CURB MARKING COLOR
- N = NEW
  - RE = REMAIN
  - RR = REMOVE & REINSTALL
  - X = REMOVE
- LENGTH IN FEET
- D38 N 103

MATCH LINE - SEE PHASE 1 IMPROVEMENT SET FOR DETAIL

MATCH LINE - SEE SHEET C4.5

**KEYNOTES**

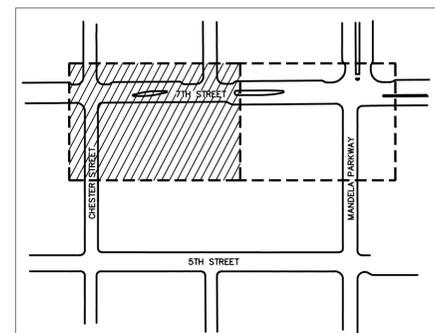
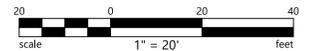
- RELOCATE EXISTING STOP SIGN AND POST
- PROPOSED STOP SIGN (CA MUTCD R1-1) WITH "ALL WAY" PLAQUE (CA MUTCD R1-3P) AND POST - SEE DETAIL 1&2/C8.0
- 4" YELLOW LINES DIAGONALS AT 10" MAXIMUM O.C.
- 4" WHITE LINES DIAGONALS AT 3" MAXIMUM O.C.
- PARKING STALL TEE - SEE DETAIL 1/C8.6
- INSTALL "NO RIGHT ON RED" SIGN (CA MYTCD R10-11) AND POST - SEE DETAIL 1&2/C8.0

**ABBREVIATIONS**

- LL LIMIT LINE - SEE DETAIL 2/C8.6
- CCW CONTINENTAL CROSS WALK - SEE DETAIL 4/C8.11
- 4"WL 4-INCH WHITE LINE
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Know what's below.  
Call before you dig.

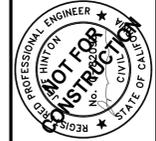


KEY PLAN  
NTS

Revisions	No.	Date	Scale	Design	Drawn	Approved	Job No.
		03/23/2026	AS SHOWN	J.M.	K.T.	J.H.	C80384

Drawing Number:  
**C4.4**  
14 OF 60

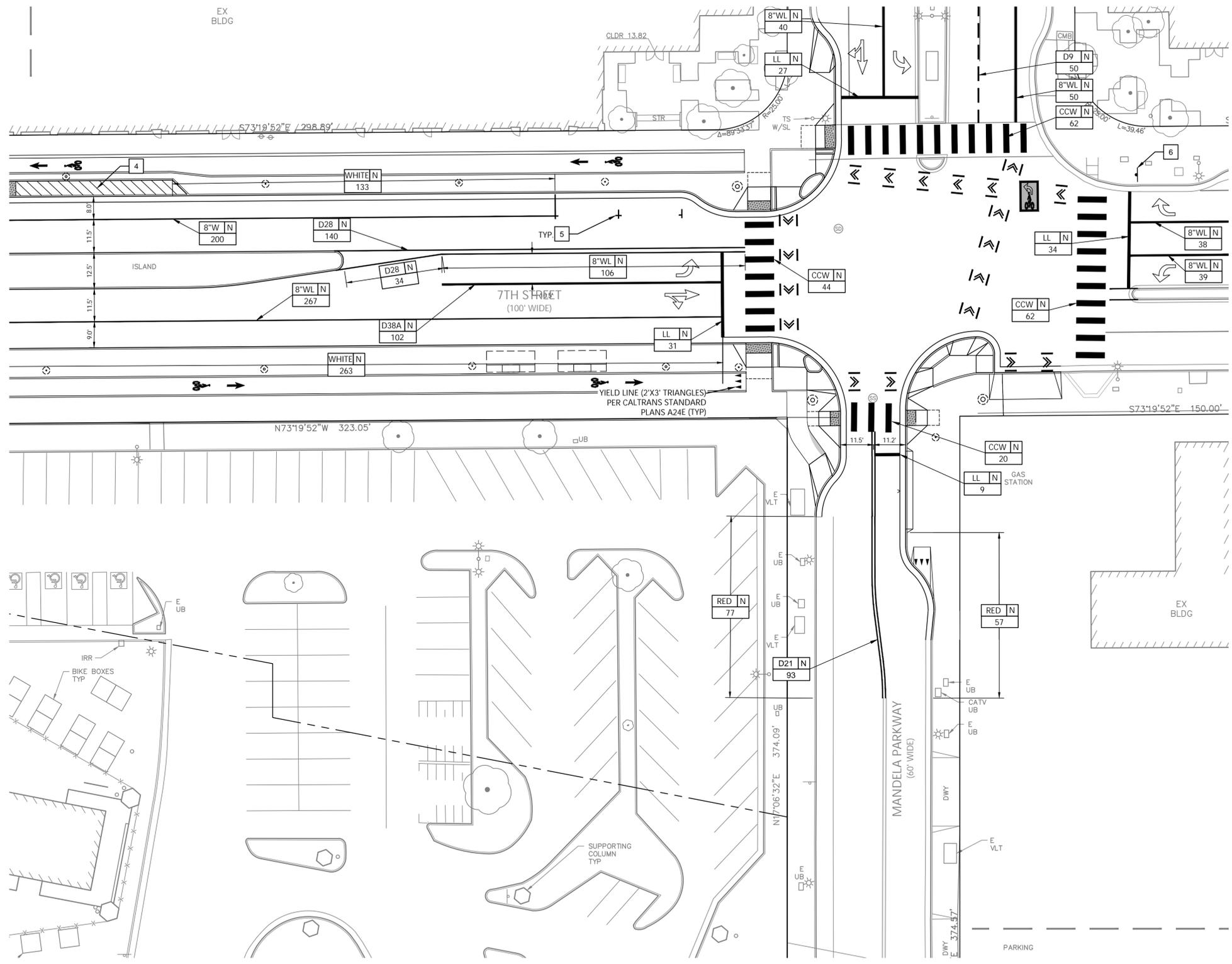
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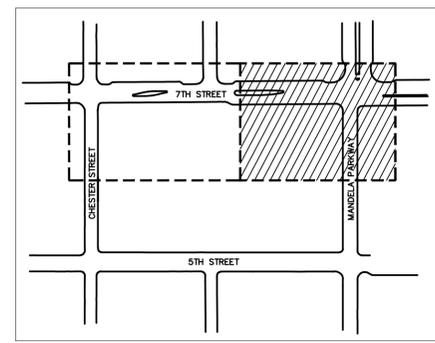
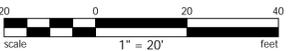
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Call before you dig.



KEY PLAN  
NTS

Revisions	No.	Date	Scale	Design	Drawn	Approved	Job No.
		03/23/2026	AS SHOWN	J.M.	K.T.	J.H.	C180384

Plot Date: 2/20/2026 at 2:12pm K:\2018\180384\_west\_oakland\_bart\_station\_public\_improvement\Phase 206\_wobart\_01\_1\_P12.dwg

## SAN FRANCISCO BAY AREA RAPID TRANSIT DISTRICT

---

### MEMORANDUM

**TO:** BART Bicycle Advisory Task Force

**FROM:** Heath Maddox, Customer Access & Accessibility

**DATE:** March 23, 2026

**SUBJECT:** Removal of keyed bicycle lockers from BART stations

The purpose of this memo is to update the BBATF on the planned removal of BART's obsolete keyed bicycle lockers.

#### DISCUSSION

For nearly 20 years, BART has been gradually phasing out its older, keyed bicycle lockers in favor of on-demand, electronic BikeLink lockers. In 2008, BART had 1,000 keyed lockers. In 2026, there are a total of 334 remaining at 15 stations, 254 of which are still operational and 120 of which are rented. Over the years, BART has kept some of its keyed lockers because demand has historically been steady, but recently, especially since 2019, demand has dwindled in favor of BART's 1,900 more modern, secure and flexible BikeLink eLockers.

Keyed lockers are an inefficient and outdated method of storing bikes at transit stations, and BART's depleted inventory of keyed lockers is now several decades old, well past its useful life. Due to their age, repairing BART's keyed lockers often requires custom fabricating parts that are no longer available. Coupled with low demand, the expense of maintaining and re-keying BART's keyed lockers has led BART's locksmiths to leave 25% of them un-repaired. Despite the low usage and deferred maintenance, BART Customer Service estimates that the keyed locker program costs approximately \$30,000 per year to administer and brings in under \$6,000 in rental income.

Considering the aged keyed lockers' condition, low renter demand, and high cost of maintenance, BART Customer Access & Accessibility and BART Customer Services concur that removal is the best option. Bike access to BART and secure bike parking utilization mirrors BART ridership, leaving ample capacity in the District's eLockers to accommodate all BART's current keyed locker renters, although some relocation of BikeLink lockers will be necessary to ensure adequate levels of secure bike parking at all stations. BART staff will begin reaching out to keyed locker renters in April, setting a move-out deadline of May 30<sup>th</sup>, 2026, with removal occurring from June through August.

# SAN FRANCISCO BAY AREA RAPID TRANSIT DISTRICT

---

## MEMORANDUM

**TO:** Board of Directors **DATE:** February 27, 2026  
**FROM:** Boris Lipkin, Assistant General Manager, Planning & Development  
**SUBJECT:** Accessibility Update: Fare Gate Tones & March 25 Orientation Event

This memo provides an update on efforts to improve accessibility for blind and low-vision customers.

(1) **Improved fare gate tones – systemwide rollout:** During the first week of March, BART will update fare gates systemwide to feature new “chime” tones that will replace the beeps that currently let blind and low vision customers know the fare gate is open. The new tones were piloted at the Ashby, Rockridge, and Civic Center stations beginning in October 2025 (the Board was informed of the pilot via a previous memo on September 11<sup>th</sup>, 2025). Feedback collected from the BART Accessibility Task Force and an online survey indicated the new tones are more pleasing overall and easier to hear above station background noise. Some customers were unaware the fare gates could generate the tones and suggested BART conduct additional outreach to inform the public how to generate the tones by holding the Clipper card on the reader for an extra second. BART Communications will create helpful content about the new tones and conduct news media outreach. Staff will continue to share this content with stakeholders.

(2) **Orientation event for blind & low vision customers:** On March 25 from 12:00-1:30 PM, BART will be hosting an orientation event for blind and low-vision customers at the 19<sup>th</sup> Street Oakland Station. Customers will be able to explore an out-of-service train at their own pace while BART staff demonstrate features such as Braille car identification numbers. Customers will also be able to navigate the vending machines and fare gates (including exploring the new “chime” tones) with plenty of staff on hand to answer questions and provide information.

Please contact me at [boris.lipkin@bart.gov](mailto:boris.lipkin@bart.gov) or (510) 506-5850 if you have any questions.

cc: Board Appointed Officers  
General Manager  
Executive Staff

# District Announcements

A message from BART Communications



Let's go.

## **BART's Next Generation Fare Gates Project wins two prestigious honors**

BART's transformative Next Generation Fare Gates Project is being recognized by two prestigious national organizations. The American Public Works Association named the fare gates as its winner of the 2026 Public Works Project Award and the International Partnering Institute announced the gates as its 2026 Collaborative Project of the Year. In August 2025, BART completed the replacement of 715 gates in all 50 stations across a system that spans five counties. The project was completed four months ahead of schedule.

"No other transit agency in the world has fare gates quite like these," said BART Assistant General Manager for Infrastructure Delivery and head of the fare gates project Sylvia Lamb. "This project is a true example of a BART-wide effort with every department contributing to the successful installation of new fare gates at every station."



There are several examples of how Next Generation Fare Gates have transformed the rider experience:

- The number of riders who tell BART they've witnessed fare evasion has dropped more than 50% compared with before the start of installation.
- New gates and BART Police Department's increased visible presence are critical parts of a comprehensive approach to rider safety that has resulted in a 41% drop in BART's overall crime rate.

- Hours spent on corrective maintenance inside BART stations for vandalism, graffiti, and broken items decreased 961 hours in the first six months after all stations had new gates.
- Early indications are the gates are responsible for BART fare revenue growing by about \$10 million annually through reduced fare evasion.

The gates feature a unique door locking mechanism that makes their swing barriers very hard to push through, jump over, or maneuver under. The overall fare gate array height (gate, console, integrated barrier) forms a barrier of 72 inches minimum to deter fare evasion.

Learn more about BART's Next Generation Fare Gates Project [here](#).

**District Announcements | March 16, 2026**  
San Francisco Bay Area Rapid Transit District

**From:** Heath Maddox  
**Sent:** Friday, March 6, 2026 10:26 AM  
**To:** [REDACTED]  
**Cc:** Bart Webcustomerservices; Heath Maddox  
**Subject:** RE: Case 00381472: Bike straps [ ref:!00Dd00hrYV.!500VI0tWoEw:ref ]

Dear Kim,

Thanks for your comment regarding the bicycle straps onboard BART trains, and thanks for biking to BART.

We are aware of the strap length issue, especially for bikes with panniers, but unfortunately, we are unable use longer straps. To better accommodate more than two bikes and/or wider or loaded bikes, our initial design for the straps was in fact a good deal longer, but we were ultimately unable to implement these longer straps due to the high likelihood the straps would be sucked onto and obstruct the air intake grate which is located immediately below the bike lean bar on all BART's rail cars. Due to the very real potential for compromising the climate control and air filtration system on the cars, having longer straps was unfortunately non-negotiable with our car engineers. We tried a number of different buckle and strap configurations to address the issue while still providing sufficient length for wider bikes, but were ultimately unsuccessful.

I have had discussions with BART's Bicycle Advisory Task Force about developing a "bring your own" strap that's a little longer and can be distributed through the BART Railgoods store (<https://www.railgoods.com/bart/>) for low to no cost, but can't offer a timeline for when it might be available. In the meantime, if this is a regular need, I'd suggest bringing your own bungee or simple nylon strap with buckle to supplement the BART straps.

Sincerely,

Heath Maddox  
Manager of Bicycle Access Programs  
Bay Area Rapid Transit District  
2150 Webster Street, 8th Floor  
Oakland, CA 94612  
415.728.1352

-----Original Message-----

From: Webcustomerservices <webcustomerservices@bart.gov>  
Sent: Friday, March 6, 2026 10:10 AM  
To: Heath Maddox <hmaddox@bart.gov>  
Subject: RE: Case 00381472: Bike straps [ ref:!00Dd00hrYV.!500VI0tWoEw:ref ]

Hi Heath,

I'm not sure if you're the decision-maker when it comes to bike straps on the trains. If not, could you please point me to the appropriate contact person or department? Please see below.

Regards,

BART Customer Services

M-F 8am to 5pm

510-464-7134

=====

Contact Name    Kim Anderson

Contact Email    [REDACTED]

Contact Phone    [REDACTED]

Opened Date/Time 3/6/2026 7:10 AM

Description    Hello, I use Bart & bike to commute to/from work, and a lot of other people on my train do as well. But the straps are only long enough to strap down 2 bikes (and that can be difficult; sometimes 1 if it's a big bike). Can you please extend the straps so more bikes can fit? (My Bart ride is 40 mins - too long to stand and hold my bike.) Thanks!  
ref:!00Dd00hrYV.!500VI0tWoEw:ref

**From:** Heath Maddox  
**Sent:** Wednesday, January 21, 2026 3:15 PM  
**To:** [REDACTED]  
**Cc:** Jumana Nabti; Webcustomerservices; Javier Panzar  
**Subject:** RE: Case 00378052: Bike Channel Stairwell Information [ ref:!00Dd00hrYV.!500VI0qk5i:ref ]

Dear Landon,

Thanks very much for your thoughtful note regarding BART's bicycle stairway channels.

I agree that we could do a better job of communicating to our customers which stations and which stairways have bike channels. It's easy enough to add that information to the "Bikes" section of our individual station pages and I'll work with our webmasters to get that done. I think to add more detail like dimensions and photos, we'd need to link to a separate page, something I'll consider.

Sincerely,

Heath Maddox  
Manager of Bicycle Access Programs  
Bay Area Rapid Transit District  
2150 Webster Street, 8th Floor  
Oakland, CA 94612  
415.728.1352

-----Original Message-----

From: Webcustomerservices <webcustomerservices@bart.gov>  
Sent: Wednesday, January 21, 2026 2:44 PM  
To: Heath Maddox <hmaddox@bart.gov>; Javier Panzar <javier.panzar@bart.gov>  
Cc: Jumana Nabti <JNabti@bart.gov>  
Subject: RE: Case 00378052: Bike Channel Stairwell Information [ ref:!00Dd00hrYV.!500VI0qk5i:ref ]

Hello Heath/Javier,

Please review customer email and feedback below.

Regards,

BART Customer Services

=====

Contact Name     Landon Kan Huy

Contact Email [REDACTED]

Contact Phone [REDACTED]

Opened Date/Time 1/16/2026 12:15 AM

Description Hello,

I'm a frequent user of BART, combining it with bikes, both cargo and conventional, quite often. I noticed the available information regarding stairway channels has not been updated in a while on the website, for example not reflecting that 16th St. Mission now has channels, or at least the pages are not yet consistent or easily accessible.

I also wanted to request accompanying information regarding stations with stairwell channels; information about which exits at stations have channels, as well as their widths (4in wide vs 11in wide) and pictures of the channels if possible, as I've noticed not all the channels are the same and some bikes may not be compatible. This would be helpful, as I've avoided trips on BART as if I wasn't sure if my origin/destination station would provide me a comfortable experience entering/exiting the system.

I applaud the changes BART has made to improve the rider experience for passengers with bicycles, and because of the changes in recent years, I definitely am encouraged to ride BART more often.  
ref:!00Dd00hrYV.!500VIOqxk5i:ref